



Board of Adjustment Staff Report

Meeting Date: June 14, 2024

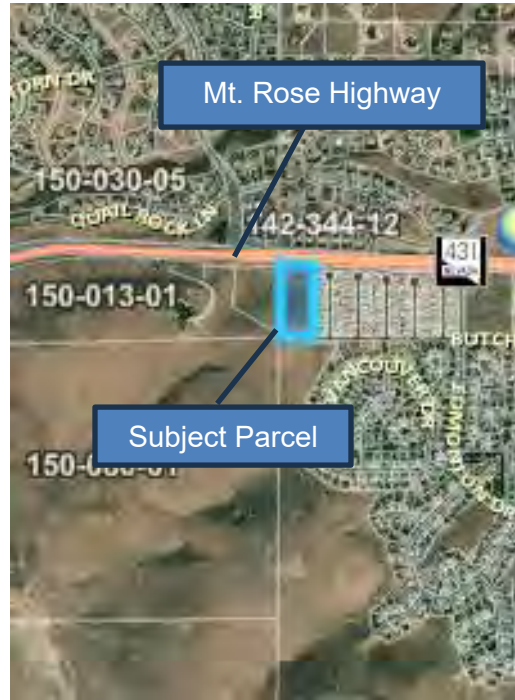
Agenda Item: 7D

SPECIAL USE PERMIT CASE NUMBER:	WSUP23-0027 (Supreme Storage)
BRIEF SUMMARY OF REQUEST:	Request for approval of a special use permit for grading and the construction of a personal storage facility
STAFF PLANNER:	Tim Evans, Planner Phone Number: 775.328.2314 E-mail: TEvans@washoecounty.gov

CASE DESCRIPTION

For hearing, discussion, and possible action to approve a special use permit for the construction of a 583-unit storage facility, and an onsite office space, for personal storage comprised of two (2) structures totaling 76,550 square feet and associated grading. Proposed ground disturbance to construct the facility is 2.55 acres, with approximately 9,234 cubic yards of cut, 7,155 cubic yards of fill, and 2,079 cubic yards of imported material.

Applicant: Joe Strobele
 Property Owner: Winterberg 2015 Family Trust
 Location: 3900 Mount Rose Highway,
 Reno, NV 89511
 APN: 049-402-01
 Parcel Size: 5.026 acres
 Master Plan: Commercial
 Regulatory Zone: Neighborhood Commercial (NC)
 Area Plan: Forest
 Development Code: Authorized in Article 810,
 Special Use Permits
 Commission: 2 – Commissioner Clark
 District:



Vicinity Map

STAFF RECOMMENDATION

APPROVE

APPROVE WITH CONDITIONS

DENY

POSSIBLE MOTION

I move that, after giving reasoned consideration to the information contained in the staff report and information received during the public hearing, the Washoe County Board of Adjustment approve with conditions Special Use Permit Case Number WSUP23-0027 for Supreme Storage, with the conditions included as Exhibit A to this matter, having made all five findings in accordance with Washoe County Code Section 110.810.30.

(Motion with Findings on Page 21)

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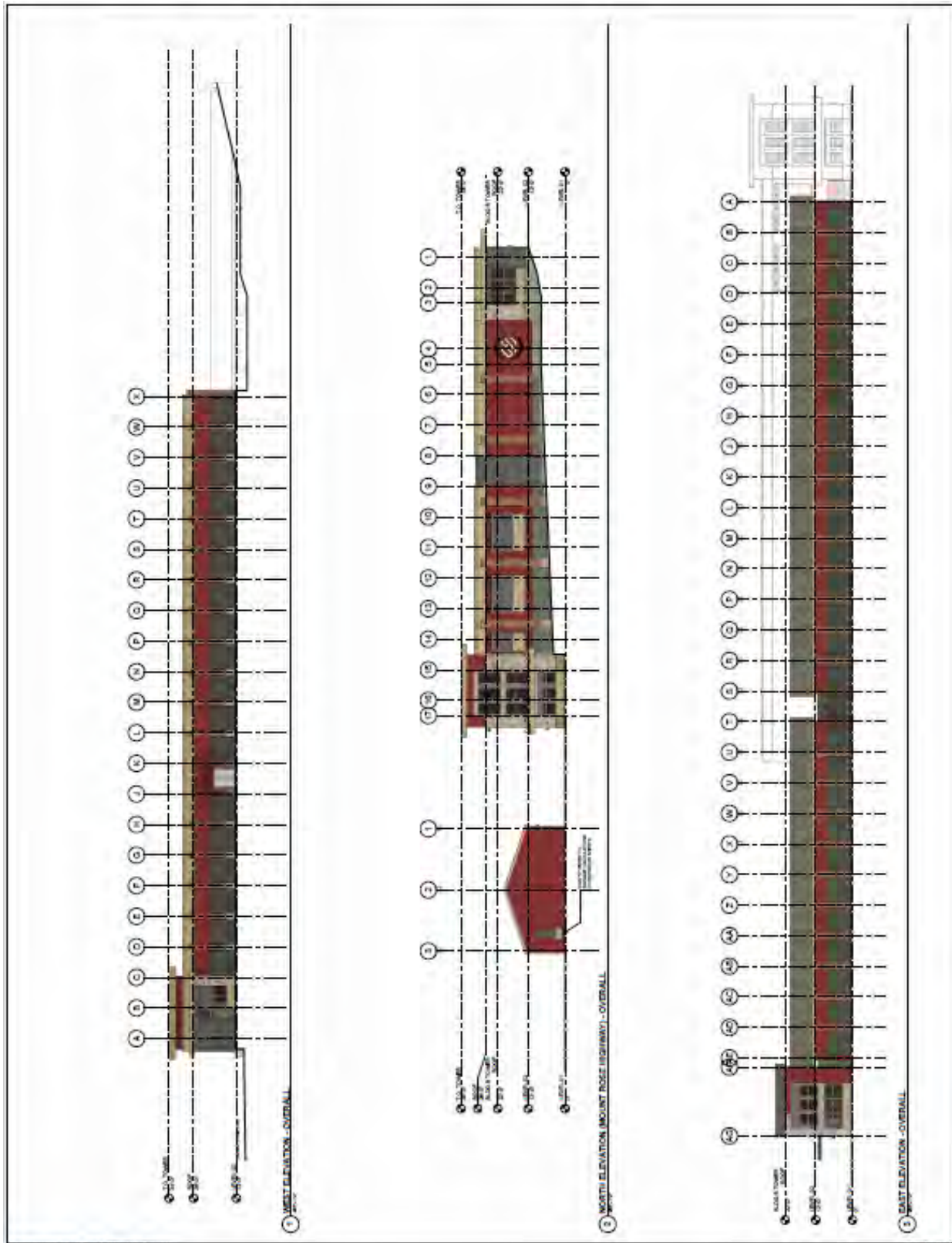
Special Use Permit

The purpose of a special use permit is to allow a method of review to identify any potential harmful impacts on adjacent properties or surrounding areas for uses that may be appropriate within a regulatory zone; and to provide for a procedure whereby such uses might be permitted by further restricting or conditioning them so as to mitigate or eliminate possible adverse impacts. If the Board of Adjustment grants an approval of the special use permit, that approval is subject to conditions of approval. Conditions of approval are requirements that need to be completed during different stages of the proposed project. Those stages are typically:

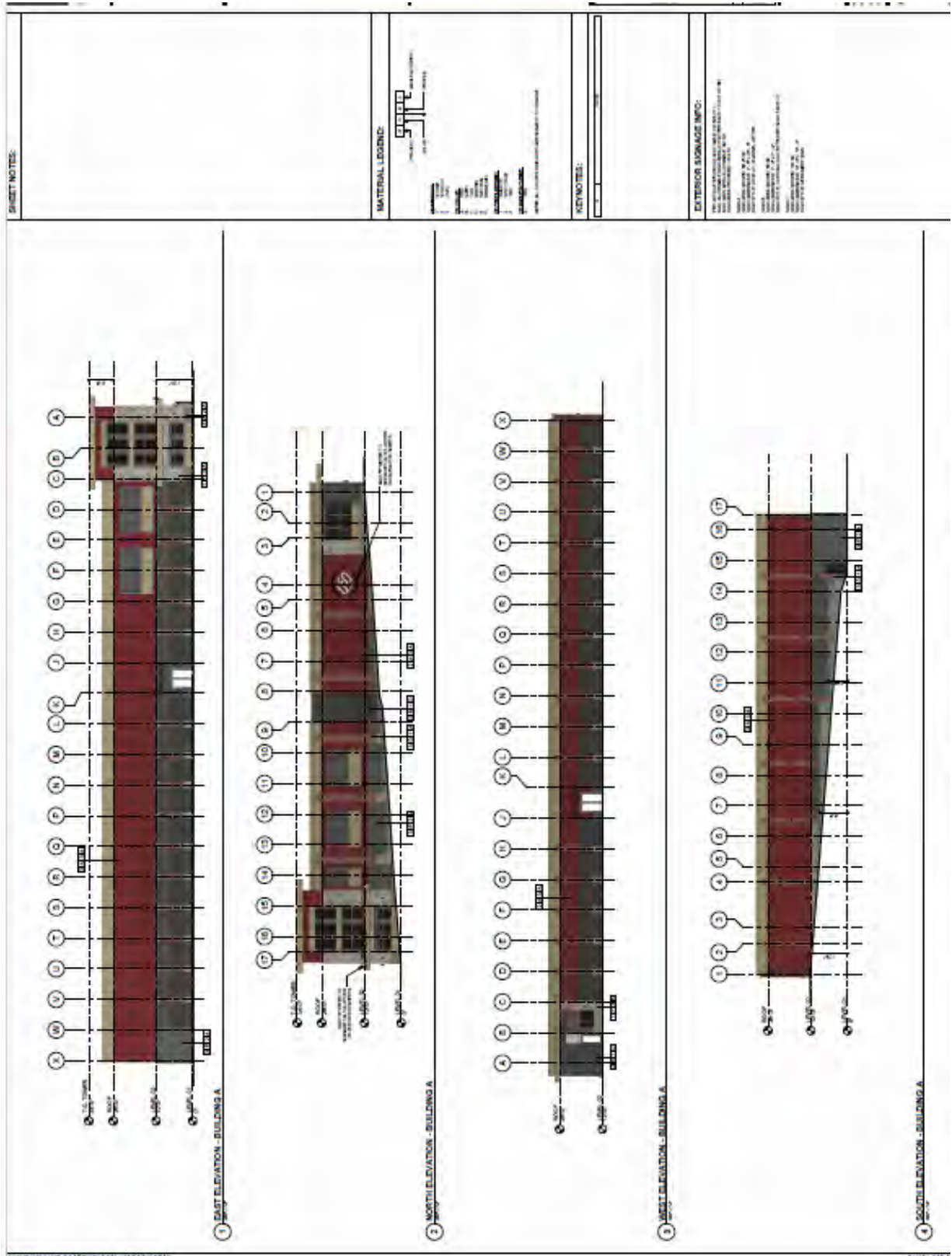
- Prior to permit issuance (i.e. a grading permit, a building permit, etc.)
- Prior to obtaining a final inspection and/or a certificate of occupancy on a structure
- Prior to the issuance of a business license or other permits/licenses
- Some conditions of approval are referred to as “operational conditions.” These conditions must be continually complied with for the life of the business or project.

The conditions of approval for Special Use Permit Case Number WSUP23-0027 are attached to this staff report and will be included with the action order.

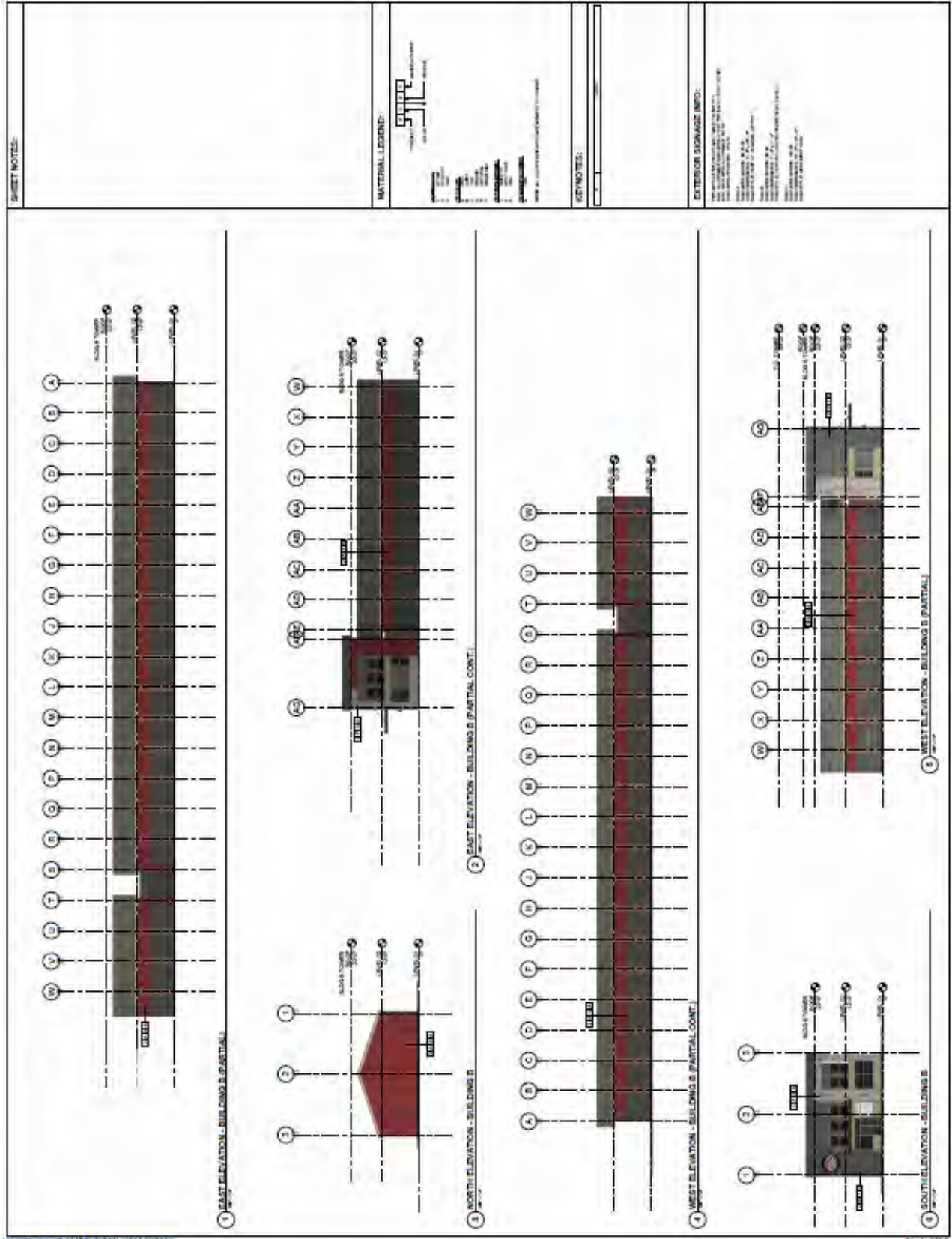
The subject property is designated as Neighborhood Commercial (NC). The proposed use of a 583-unit storage facility which is classified as a personal storage use type is permitted in the NC regulatory zone with a special use permit per WCC 110.302.05, Table 110.302.05.3. Additionally, the proposed grading will trigger the major grading thresholds per WCC 110.438.35. Therefore, the applicant is seeking approval of this SUP from the Board of Adjustment.



Elevations



Elevations



Elevations



Renderings Looking South

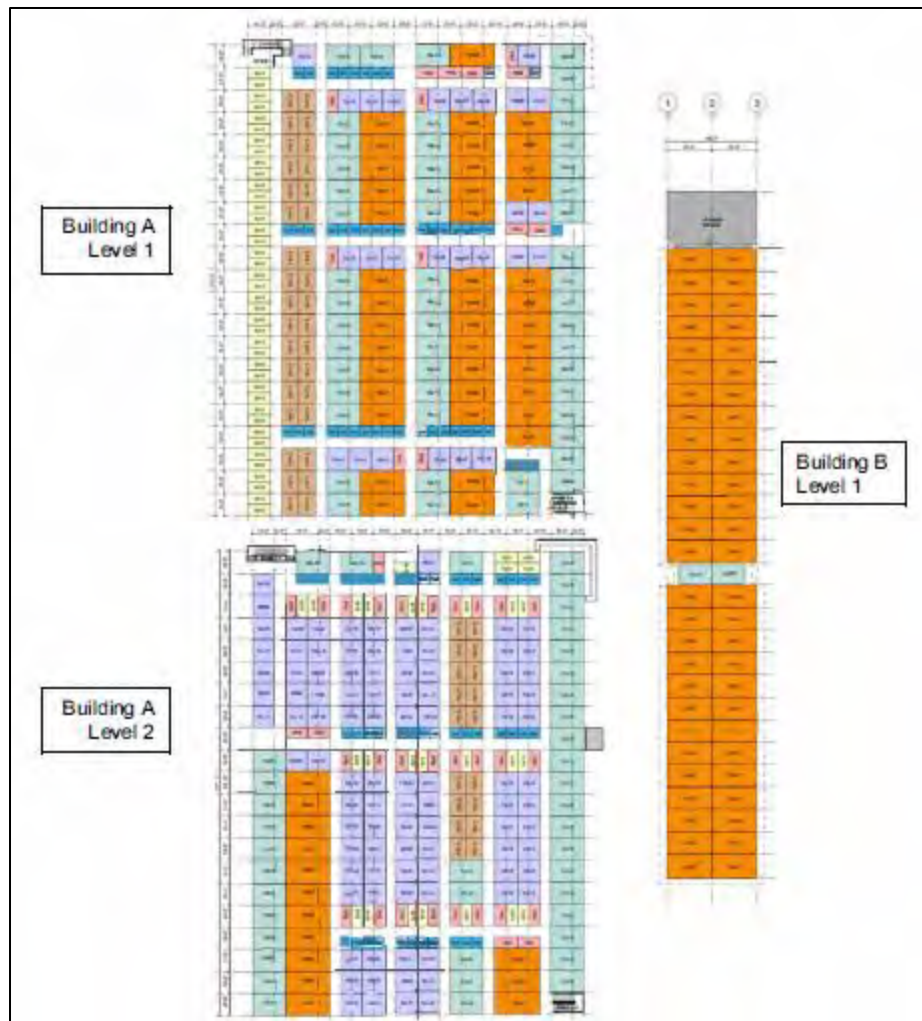


Renderings Looking Southwest

Project Evaluation

The applicant, Joe Strobele, is requesting a special use permit for the construction of a 583-unit storage facility (see figure below), and an onsite office space, for personal storage comprised of two (2) structures totaling 76,550 square feet. Proposed ground disturbance to construct the facility is 2.55 acres, with approximately 6,871 cubic yards of cut, 7,039 cubic yards of fill, and 168 cubic yards of imported material. The figure below shows the floor plan of the proposed storage facility buildings.

Due to extenuating circumstances, the applicant is asking the Board of Adjustment to grant a seven-year approval of the special use permit. The additional time is needed to accommodate the anticipated timeline identified by the Regional Transportation Commission to complete the Butch Cassidy extension project which is a requirement for the successful completion of this project. Refer to condition 1(c) in Exhibit A (Conditions of Approval).



The subject parcel (APN 049-402-01) is 5.026 acres and is zoned Neighborhood Commercial (NC). The property will be accessed via Mt. Rose Highway and Butch Cassidy Drive, Truckee Meadows Water Authority (TMWA) for water, Truckee Meadow Water Reclamation Facility (TMWRF) for sewer, NV Energy for electricity, and Truckee Meadows Fire Protection District (TMFPD) for fire protection.

Article 204 – Forest Area

Pursuant to WCC Section 110.204.05, *Mt. Rose Highway Scenic Roadway Corridor Standards*, subsection (c)(1) states the following:

Setback. A minimum setback of thirty (30) feet shall be provided along any property line adjoining the Mt. Rose Highway.

(1) Structures shall not be permitted within the setback area. A freestanding sign is not considered a structure.

The proposed storage facility, per the site plan on page 4, is setback over thirty (30) feet from the front property line.

Pursuant to WCC Section 110.204.25, *Mt. Rose Highway Commercial Modifiers*, subsection (c) states the following:

(c) Ingress, egress, and internal circulation must be designed to improve overall traffic safety, improve access for affected adjacent property owners, consolidate and minimize access to State Route 431, promote pedestrian and cycling activity, and mitigate any negative impact to existing development.

The project information was provided to Washoe County Engineering, the Nevada Department of Transportation (NDOT), and the Regional Transportation Commission (RTC). Conditions of approval addressing access to the proposed storage facility from Washoe County Engineering, NDOT, and RTC are included in Exhibit A.

Article 406 - Building Placement Standards

Pursuant to WCC Section 110.406.05, General, Table 110.406.05.1, Part Three: Yard and Setback Dimensions, the setbacks for the NC regulatory zone are:

Front and Sides: Fifteen (15) feet

Rear: Twenty (20) feet

The proposed setbacks for the storage facility, as indicated on the site plan on page 4, comply with the required setbacks of the NC regulatory zone.

Article 410 - Parking

Pursuant to Washoe County Code (WCC) Section 110.410.10.3, Off-street Parking Space Requirements (Commercial Use Types), the following is required:

- 1 space / employee during peak employment shift
 - Applicant states that there will be two (2) full-time employees. Therefore, two (2) parking spaces are required.
- 1 loading space for loading per 2,000 square feet of building footprint
 - 44,425 square foot building footprint for both buildings requiring 23 parking spaces.

Pursuant to WCC 110.410.15.1, Handicapped Accessible Spaces, the following is required:

- 1 handicapped accessible space for 1-25 required
 - 1 handicapped space for twenty-five (25) parking spaces is required.

Total parking spaces required: Twenty-six (26) spaces, including one (1) handicapped accessible parking space.

Per the site plan on page 4, seven (7) striped parking spaces, which includes one (1) handicapped accessible space and one (1) van accessible parking space, will be provided. Additionally, due to the proposed project being a storage facility, customers of the storage facility, in order to reduce the distance travelled to their vehicle, will park vehicles in front of the bay doors while transporting items to and from their storage area. The areas in front of the bay doors will serve as the required loading parking spaces, which totals to eighty (80) loading spaces.

Customers would park their vehicles parallel to the bay door and load/unload items. Pursuant to WCC Section 110.410.25.1, a parallel parking space shall be twenty-two (22) feet long and eight and one-half (8.5) feet wide, with a minimum maneuvering aisle width of twelve (12) feet. Per the civil drawings included with the application (Exhibit D), the proposed project meets the requirements of Article 410, *Parking and Loading*, for parking and maneuvering aisles.

Article 412 Landscaping

Pursuant to Washoe County Code Section 110.412.40(a), 20% of the developed area must be landscaped. The developed area for the project is a total of 140,988 square feet (approx. 3.24 acres). Therefore, the project must have no less than 28,198 square feet (approx. 0.65 acres) of landscaping. The landscaping plan proposes 28,198 square feet of landscaping which meets this

requirement. Landscape areas include buffer areas, streetscapes, parking lot edges and islands and various planter beds.

Pursuant to WCC Section 110.412.40(b), *Required Yards Adjoining Streets*, requires one (1) tree for every fifty (50) feet, or fraction thereof, of street frontage. The property has 320 feet of street frontage which requires seven (7) trees to be planted along the street frontage – the applicant proposes fourteen (14) trees.

Pursuant to WCC Section 110.412.40(c)(1), *Landscape Buffers Adjoining Residential Uses*, the landscaped buffer area between the residential use and commercial use shall be the width of the required yard setback for the entire common property line. The property adjoins a residential use along the eastern property line, which requires the common property line to have a landscape buffer the width of the setback for the entire common property line. As detailed on the site plan on page 4, the applicant proposes a landscape buffer along the eastern property line that is the required setback width of fifteen (15) feet.

Pursuant to WCC Section 110.412.40(c)(2), *Landscape Buffers Adjoining Residential Uses*, the buffer required by WCC Section 110.412.40(c)(1) shall have one (1) tree every twenty (20) feet of common property line. The property adjoins residential uses along the eastern property line (property line length is: 690.20 feet), which requires a minimum of thirty-four (34) trees – the applicant proposes fifty-seven (57) trees.

Pursuant to WCC Section 110.412.40(d), *Screening Adjoining Residential Uses*, when a commercial use adjoins a residential use, a solid decorative fence of no less than six (6) feet shall be erected along the entire length of the common property line. The fence may be constructed of stone, concrete, metal, synthetic or vinyl, but not wood. The applicant, per the civil drawings (Exhibit D), proposes a fence along the eastern property line. A condition of approval serves to address the fence being a height of no less than six (6) feet.

Pursuant to 110.412.60(g)(5), *General*, one (1) tree and six (6) shrubs are required for every three hundred (300) square feet of planting area. The planting area is 28,198 square feet. Therefore, within the landscaped area, there must be ninety-four (94) trees and five hundred and sixty-four (564) shrubs – the applicant proposes ninety-five (95) trees and five hundred and seventy (570) shrubs.

As proposed the landscape plan meets the requirements of Article 412, *Landscaping*.

Article 414 Noise and Lighting Standards

WCC Section 110.414.21, *Light and Glare*, subsection (a) states the following:

(a) Light. All light sources shall be located and installed in such a way as to prevent spillover lighting onto adjoining properties. The following provisions shall apply to all existing and proposed development:

(1) Any lighting facilities shall be so installed as to reflect away from adjoining properties. Covers must be installed on all lighting fixtures and lamps must not extend below the bottom of the cover.

(2) Light standard in or within one hundred (100) feet of residential zones shall not exceed twelve (12) feet in height. Additional standard height may be permitted by the Director of Community Development provided such lights are a sharp cutoff lighting system.

The applicant provided the following list of lighting fixtures for the project. As detailed in the list below, all lighting fixtures will be shielded to prevent light spillover onto adjoining properties. A condition of approval serves to address light fixture height being no greater than twelve (12) feet.

Schedule											
Symbol	Label	Image	QTY	Manufacturer	Catalog	Description	Number Lamps	Lamp Output	LLF	Input Power	Polar Plot
	S1		16	Lithonia Lighting	LDMSCYL 40/20 LOGAR LD	GIN LDN CYLINDER, 4000K, 2900LM, CLEAR, MATTE DIFFUSE REFLECTOR, CR180	1	1798	0.9	22.92	
	S2		45	Lithonia Lighting	WDGE2 LED P2 40K 70CRI T2M	WDGE2 LED WITH P2 - PERFORMANCE PACKAGE, 4000K, 70CRI, TYPE 2 MEDIUM OPTIC	1	2326	0.9	18.98.5	
	S3		1	Lithonia Lighting	DSX1 LED 30C 530 40K T2M MVOLT HS MA	DSX1 LED WITH (1) 30 LED LIGHT ENGINES, RIGHT ROTATED TYPE T2M OPTIC, 4000K, @ 530mA WITH HOUSING SIDE SHIELD, FOR MAST ARM MOUNTING	1	4280	0.9	52.4	

WCC Section 110.414.21, *Light and Glare*, subsection (c) states the following:

(c) Glare. Reflected glare on nearby buildings, streets or pedestrian areas shall be avoided by incorporating overhangs and awnings, using non-reflective building materials for exterior walls and roof surfaces, controlling angles of reflection, and placing landscaping and screening in appropriate locations.

As shown in the elevations on page 5 - 7, the proposed personal storage facility will have windows facing north and east on the larger, two-story structure. A condition of approval serves to address the windows and building materials being non-reflective by incorporating tinting on the windows or similar non-reflective materials. Additionally, 1(k) will require the interior lighting to be turned off at 10:00 p.m. each night in order to reduce nighttime light glare resulting from this project.

Article 424 – Hillside Development

Per Washoe County Development Code Section 110.424.05(a)(1), properties with 15% or greater slope on 20% or more of the site is subject to the applicable requirements of Article 424, *Hillside Development*. Approximately 44% of the site has more than 15% slope – see slope map on page 12.

Comparing the slope map on page 12 and the site plan on page 4 shows the proposed development footprint has been designed to follow the existing terrain and avoid areas of the site with greater than 20% slope. As detailed on the developable area map on page 14, approximately 68% of the parcel is developable area with slopes less than 20%.

The applicant states the following in the application:

“Retaining walls have been placed in lieu of large slope cuts on site where indicated. Grading of steep slope areas on the site has been minimized as shown.

The proposed design is in conformance with the site development and grading standards in Section 110.424.30 and 110.424.35. The buildings have been designed to follow the existing terrain and minimize adverse impacts to the environment.

The proposed design will not impact any ridgelines, significant viewsheds, canyons or prominent rock outcroppings. There are no ridgelines. However, there is a prominent viewshed and prominent rock outcroppings that are preserved in this design effort.”

Therefore, the proposed project, as designed, is in conformance with the requirements of Article 424, *Hillside Development*.

Supreme Storage

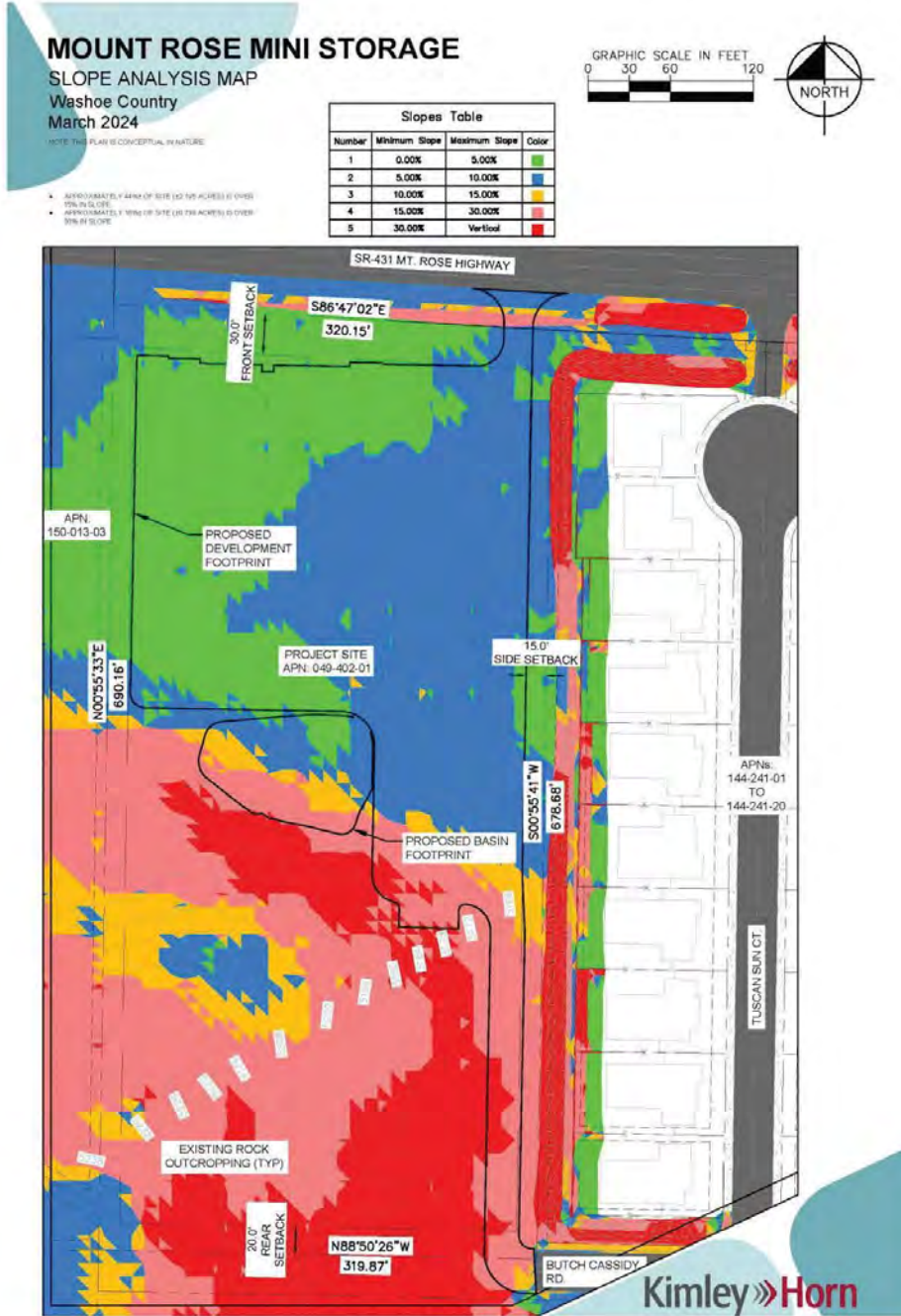


Figure 6 – Slope Map

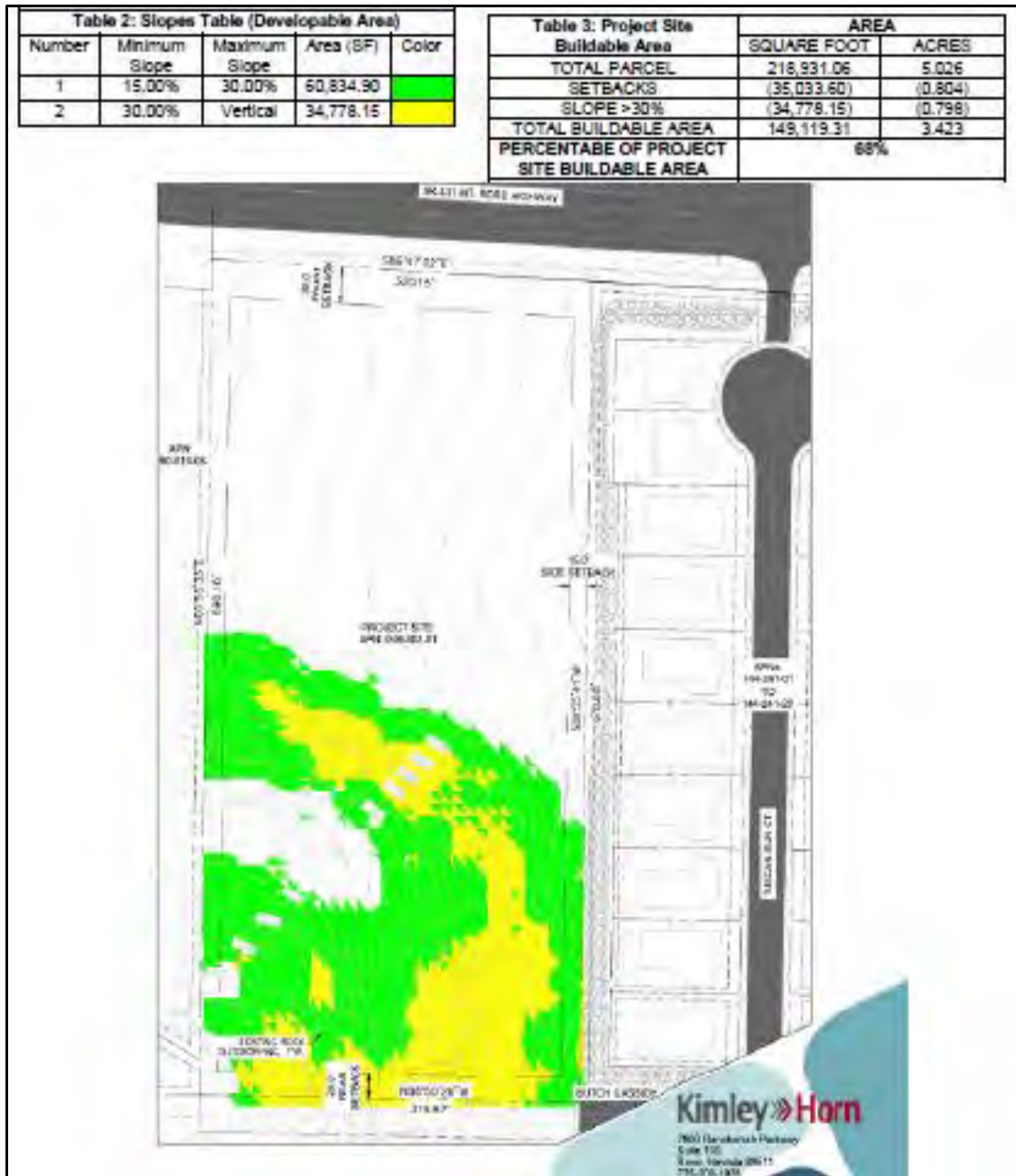
Slope Map

Article 438 - Grading

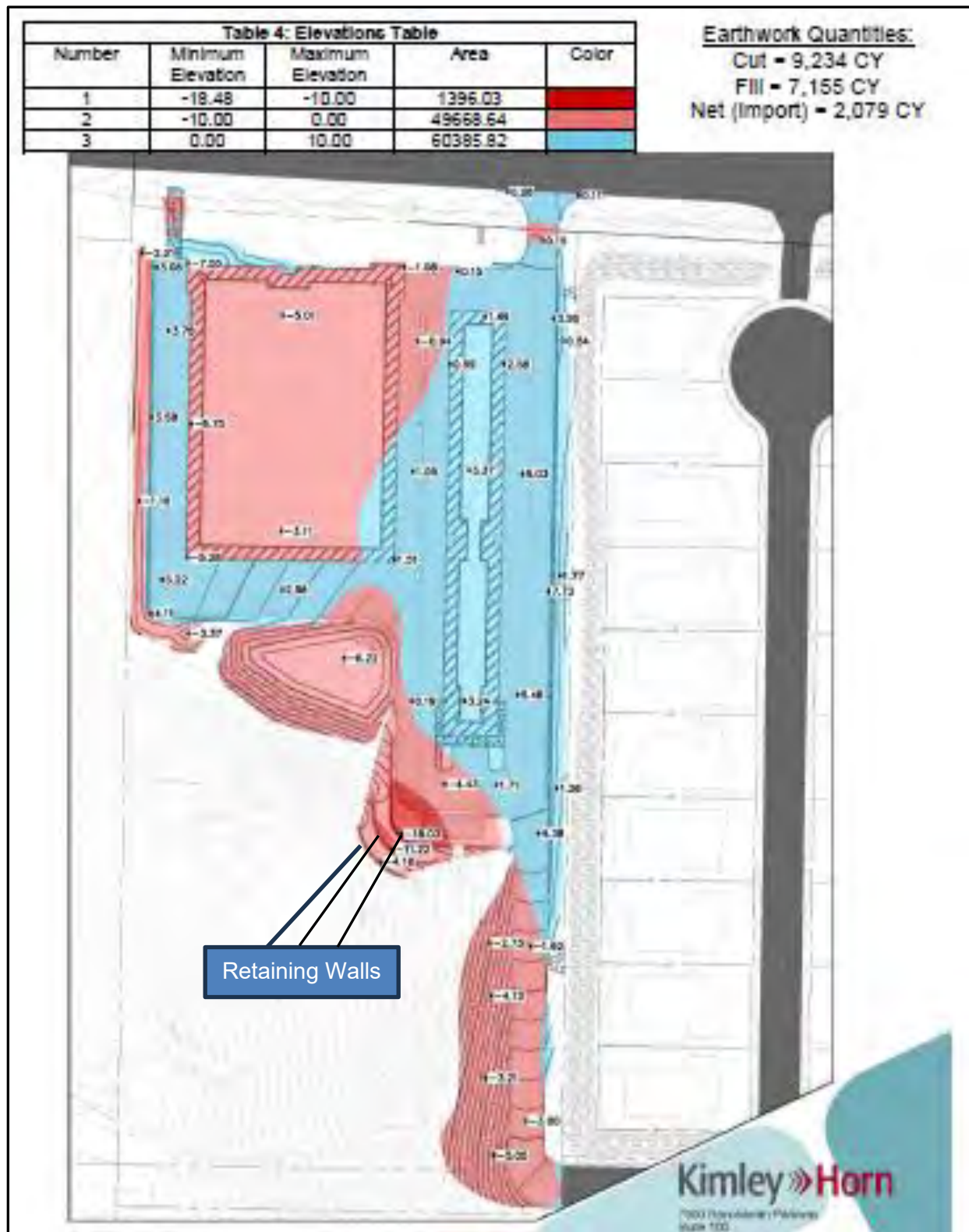
The application indicates there will be approximately 3.24 acres of surface disturbance, 9,234 cubic yards of cut, 7,155 cubic yards of fill and 2,079 cubic yards of imported material. As detailed on the site plan on page 4 and the cut and fill map on page 9, the proposed project generally avoids disturbance of the area of slopes greater than 20%. See Developable Area Map on page 8 for contours and slope analysis.

The proposed project exceeds two major grading permit thresholds in Article 438, *Grading Standards*. The thresholds are outlined below:

- **Section 110.438.35(a)(1)(i)(A)** - Grading of an area of one (1) acre (43,560 square feet) or more on parcels less than six (6) acres in size; or
- **Section 110.438.35(a)(1)(ii)(A)** - Excavation of five thousand (5,000) cubic yards or more whether the material is intended to be permanently located on the project site or temporarily stored on a site for relocation to another, final site, or;
- **Section 110.438.35(a)(2)(i)(A)** - Grading of one-half (0.5) acre (21,780 square feet) or more on parcels less than six (6) acres in size; or
- **Section 110.438.35(a)(2)(ii)(A)** – *Excavation of five thousand (5,000) cubic yards or more whether the material is intended to be permanently located on the project site or temporarily stored on a site for relocation to another, final site.*



Developable Area Map



Cut and Fill Map

All grading is proposed to have a maximum of a 3:1 slope and will be revegetated with native plant species. A ten (10) foot tall rockery or similarly engineered retaining wall is proposed in the southern portion of the site as indicated on the Cut and Fill Map above.

Article 436 - Street Design Standards

Due to the location of the project, there is potential for it to utilize the Mt. Rose Highway to the north and Butch Cassidy Drive to the south for access to the parcel. The applicant is proposing main access off of Butch Cassidy Drive and emergency access only with a gate along Mt. Rose Highway.

The Washoe County Engineering and Capital Projects Division (Washoe County Engineering), Nevada Department of Transportation (NDOT), and the Regional Transportation Commission (RTC) reviewed the proposed project.

Washoe County Engineering recommended approval of the project with conditions and commented that they do not support direct access onto Mt. Rose Highway from this proposed project due to the additional direct access causing degradation of traffic safety on the Mt. Rose highway corridor. They further explained that the proposed secondary gated emergency access is acceptable if the civil design can integrate safe access across the existing multiuse path and barrier rail along the project frontage to Mt. Rose Highway.

Washoe County Engineering provided conditions (Exhibit A) in relation to requiring coordination and approval from the Regional Transportation Commission (RTC) for integration of the Butch Cassidy Drive extension plans to eliminate conflicts with site improvements, including construction of structures, storm drainage improvements, and access roadways.

NDOT provided the following comments concerning the access to Mt. Rose Highway:

“NDOT requires the use of permitted access to the state highway system. A NDOT occupancy permit will be required for the proposed improvements within and adjacent to SR 431 right of way. The maintaining agency of the access will be required to be the permittee.

All work proposed within or adjacent to the SR 431 right of way will require an encroachment permit and must comply with NDOT’s Standard Plans, Access Management System and Standards, Terms and Conditions Relating to Right-of-Way Occupancy Permits, and the Drainage Manual current version at the time of application. Please contact the NDOT District II Permits Office at (775) 834-8330 for information about obtaining NDOT occupancy permits.”

RTC provided the following comments concerning the project:

”RTC requests continued coordination between the two projects [the proposed storage facility and connection project to Butch Cassidy Drive]. RTC has no objection to the general location of the connection to Butch Cassidy Drive. Further design coordination for items including but not limited to pedestrian access, placement of permanent structures, and drainage improvements will be needed and RTC further anticipates participation in review of building and grading permits for this project.

Consideration needs to be made for the existing multi-use trail, drainage, and barrier rail along Mt. Rose Highway that the emergency access will be crossing. ADA ramps and detectable warning tiles are anticipated to be required at the driveway crossing, driveway grading will need to meet ADA requirements, and appropriate protection/crash attenuation that meets NDOT requirements needs to be addressed for any gap in the barrier rail as a part of the NDOT encroachment permit.”

Conditions of approval (Exhibit A) serve to address the comments from Washoe County Engineering, NDOT, and RTC in relation to access to Mt. Rose Highway and Butch Cassidy Drive.

Article 505 – Signs

As detailed on the site plan on page 4 and the elevations shown on pages 5 – 7, the signage proposed for the storage facility includes a monument sign on the southwestern side of the property near the entrance of the storage facility from Butch Cassidy Drive and a sign on the southern face of building “B” and the northern face of building “A.”

Conditions of approval (Exhibit A) serve to address meeting the sign requirements of WCC Article 505, *Signs*, and a sign permit being obtained for the signage.

Neighborhood Meeting

The applicant hosted two neighborhood meetings, the first meeting was held at the South Valleys Library on June 29, 2023, and the second meeting was held via zoom on July 24th 2023. Only a few people attended the first meeting, for this reason the applicants held a second meeting in which 14 people participated, as well as Washoe County staff. The primary issues brought up at the neighborhood meetings included why that specific use was proposed, privacy concerns regarding windows on the building overlooking the existing residences, access issues, lighting, landscaping/buffer issues, hours of operation, construction duration and construction noise. The applicant has worked with NDOT, TRPA, and Washoe County throughout the application process regarding traffic concerns.

Forest Area Plan Evaluation

The subject parcel is located within the Forest Area Plan. The following is/are the pertinent policies from the Area Plan:

Relevant Area Plan Policies Reviewed

Policy	Brief Policy Description	Complies	Comment//Condition of Approvsl
Policy 1.2	Maintain dark night skies.	Yes	A condition of approval serves to address ensuring lighting does not spillover onto adjacent parcels pursuant to Washoe County Code Section 110.414.21.
Policy 4.1	Encourage design of industrial, commercial, and multifamily uses to contribute to the community’s sense of place rather than detract from it.	Yes	The proposed storage facility is designed, per the application (Exhibit D), to encourage the compatibility with the property and surrounding area.

Reviewing Agencies

The following agencies/individuals received a copy of the project application for review and evaluation.

Agencies	Sent to Review	Responded	Provided Conditions	Contact
BLM - NV State Office	X			
NDOT (Transportation)	X	X	X	Michelle Griffin, mgriffin@dot.nv.gov
NDOW (Wildlife)	X			
Washoe County Building & Safety	X			
Washoe County Parks & Open Space	X	X	X	Faye-Marie Pekar, fpekar@washoecounty.gov
Washoe County Sewer	X		X	Alex Mayorga, amayorga@washoecounty.gov
Washoe County Traffic	X	X	X	Mitch Fink, MFink@washoecounty.gov
Washoe County Water Rights Manager (All Apps)	X	X	X	Timber Weiss, tweiss@washoecounty.gov
Washoe County Engineering (Land Development) (All Apps)	X	X	X	Rob Wimer, rwimer@washoecounty.gov; Janelle Thomas, jkthomas@washoecounty.gov
Washoe County Engineering & Capital Projects Director (All Apps)	X			
WCHD Environmental Health	X	X	X	Jim English, jenglish@washoecounty.gov; Wes Rubio, wrubio@washoecounty.gov;
TMFPD	X	X	X	Dale Way, dway@tmfpd.us; Brittany Lemon,
AT&T	X	X		
NV Energy	X			
Truckee Meadows Water Authority	X			
Truckee Meadows Water Reclamation Facility (City of Sparks)	X			

Additionally, the Regional Transportation Commission (RTC) and the Nevada Division of Water Resources provided comments and conditions.

All conditions required by the contacted agencies can be found in Exhibit A, Conditions of Approval.

Staff Comment on Required Findings

WCC Section 110.810.30, Article 810, *Special Use Permits*, requires that all of the following findings be made to the satisfaction of the Washoe County Board of Adjustment before granting approval of the request. Staff has completed an analysis of the special use permit application and has determined that the proposal is in compliance with the required findings as follows.

- (a) Consistency. That the proposed use is consistent with the action programs, policies, standards and maps of the Master Plan and the Forest Area Plan.

Staff Comment: The personal storage use and associated grading is consistent with the Master Plan and Forest Area Plan, as proposed.

- (b) Improvements. That adequate utilities, roadway improvements, sanitation, water supply, drainage, and other necessary facilities have been provided, the proposed improvements are properly related to existing and proposed roadways, and an

adequate public facilities determination has been made in accordance with Division Seven.

Staff Comment: The Washoe County Engineering and Capital Projects Division provided conditions to ensure appropriate drainage facilities are constructed, as well as access and utility requirements are addressed. The Washoe County Health District provided a condition requiring the review and approval of all future building permits prior to permit issuance or facility construction. The Washoe County Water Rights Division provided a condition to require written approval or a will-serve from the Truckee Meadows Water Authority (TMWA) to provide service for the additional use on this property prior to building permit issuance.

- (c) Site Suitability. That the site is physically suitable for personal storage, and for the intensity of such a development.

Staff Comment: All proposed grading is consistent with the Washoe County Code and generally occurs in areas with slopes less than 20%. The location of the personal storage use was designed to avoid the portions of the site with the steepest slopes, to the extent possible. The Washoe County Engineering Division reviewed the application and had no concerns about site suitability.

- (d) Issuance Not Detrimental. That issuance of the permit will not be significantly detrimental to the public health, safety or welfare; injurious to the property or improvements of adjacent properties; or detrimental to the character of the surrounding area.

Staff Comment: The proposed personal storage use will not be detrimental to public health, safety, or welfare. Washoe County Engineering, the Nevada Department of Transportation, and the Regional Transportation Commission reviewed the project and provided comment to mitigate current and future access issues for the proposed project. Landscaping proposed will mitigate any negative visual impacts to adjoining properties and public roadways. No public comment has been received on the application.

- (e) Effect on a Military Installation. Issuance of the permit will not have a detrimental effect on the location, purpose or mission of the military installation.

Staff Comment: There is no military installation within the area of required notice for this special use permit; therefore, the project will have no effect on a military installation.

Recommendation

After a thorough analysis and review, Special Use Permit Case Number WSUP23-0027 is being recommended for approval with conditions. Staff offers the following motion for the Board's consideration.

Motion

I move that, after giving reasoned consideration to the information contained in the staff report and information received during the public hearing, the Washoe County Board of Adjustment approve with conditions Special Use Permit Case Number WSUP23-0027 for Joe Strobele, CPD Mt. Rose LLC, with the conditions included as Exhibit A to this matter, having made all five findings in accordance with Washoe County Code Section 110.810.30:

- (a) Consistency. That the proposed use is consistent with the action programs, policies, standards and maps of the Master Plan and the Forest Area Plan;

- (b) Improvements. That adequate utilities, roadway improvements, sanitation, water supply, drainage, and other necessary facilities have been provided, the proposed improvements are properly related to existing and proposed roadways, and an adequate public facilities determination has been made in accordance with Division Seven;
- (c) Site Suitability. That the site is physically suitable for personal storage, and for the intensity of such a development;
- (d) Issuance Not Detrimental. That issuance of the permit will not be significantly detrimental to the public health, safety or welfare; injurious to the property or improvements of adjacent properties; or detrimental to the character of the surrounding area;
- (e) Effect on a Military Installation. Issuance of the permit will not have a detrimental effect on the location, purpose or mission of the military installation.

Appeal Process

Board of Adjustment action will be effective ten (10) calendar days after the written decision is filed with the Secretary to the Board of Adjustment and mailed to the applicant, unless the action is appealed to the Washoe County Board of County Commissioners, in which case the outcome of the appeal shall be determined by the Washoe County Board of County Commissioners. Any appeal must be filed in writing with the Planning and Building Division within 10 calendar days from the date the written decision is filed with the Secretary to the Board of Adjustment and mailed to the applicant.

Applicant: Joe Strobele, Managing Member, CPD Mt Rose, LLC.
jstrobele@cp-investments.com

Property Owner: Winterberg 2015 Family Trust; 5395 Goldenrod Dr., Reno NV 89511

Representative: John Krmptic, KLS Planning & Design; johnk@klsdesigngroup.com



Conditions of Approval

Special Use Permit Case Number WSUP23-0027

The project approved under Special Use Permit Case Number WSUP23-0027 shall be carried out in accordance with the conditions of approval granted by the Board of Adjustment on June 14, 2024. Conditions of approval are requirements placed on a permit or development by each reviewing agency. These conditions of approval may require submittal of documents, applications, fees, inspections, amendments to plans, and more. These conditions do not relieve the applicant of the obligation to obtain any other approvals and licenses from relevant authorities required under any other act.

Unless otherwise specified, all conditions related to the approval of this special use permit shall be met or financial assurance must be provided to satisfy the conditions of approval prior to issuance of a grading or building permit. The agency responsible for determining compliance with a specific condition shall determine whether the condition must be fully completed or whether the applicant shall be offered the option of providing financial assurance. All agreements, easements, or other documentation required by these conditions shall have a copy filed with the County Engineer and the Planning and Building Division.

Compliance with the conditions of approval related to this special use permit is the responsibility of the applicant, his/her successor in interest, and all owners, assignees, and occupants of the property and their successors in interest. Failure to comply with any of the conditions imposed in the approval of the special use permit may result in the institution of revocation procedures.

Washoe County reserves the right to review and revise the conditions of approval related to this Special Use Permit should it be determined that a subsequent license or permit issued by Washoe County violates the intent of this approval.

For the purpose of conditions imposed by Washoe County, “may” is permissive and “shall” or “must” is mandatory.

Conditions of approval are usually complied with at different stages of the proposed project. Those stages are typically:

- Prior to permit issuance (i.e., grading permits, building permits, etc.).
- Prior to obtaining a final inspection and/or a certificate of occupancy.
- Prior to the issuance of a business license or other permits/licenses.
- Some “conditions of approval” are referred to as “operational conditions.” These conditions must be continually complied with for the life of the project or business.

The Washoe County Commission oversees many of the reviewing agencies/departments with the exception of the following agencies.

- **The DISTRICT BOARD OF HEALTH, through the Washoe County Health District, has jurisdiction over all public health matters in the Health District. Any conditions set by the Health District must be appealed to the District Board of Health.**
- **The REGIONAL TRANSPORTATION COMMISSION (RTC) is directed and governed by its own board. Therefore, any conditions set by the Regional Transportation Commission must be appealed to that Board.**

FOLLOWING ARE CONDITIONS OF APPROVAL REQUIRED BY THE REVIEWING AGENCIES. EACH CONDITION MUST BE MET TO THE SATISFACTION OF THE ISSUING AGENCY.

Washoe County Planning and Building Division

1. The following conditions are requirements of Planning and Building, which shall be responsible for determining compliance with these conditions.

Contact Name – Tim Evans, Planner, 775.328.2314, TEvans@washoecounty.gov

- a. **The applicant shall attach a copy of the action order approving this project to all permits and applications (including building permits) applied for as part of this special use permit.**
- b. The applicant shall demonstrate substantial conformance to the plans approved as part of this special use permit.
- c. The applicant shall submit construction plans, with all information necessary for comprehensive review by Washoe County, and initial building permits shall be issued within seven years from the date of approval by Washoe County. The applicant shall complete construction within the time specified by the building permits.
- d. A note shall be placed on all construction drawings and grading plans stating:

NOTE

Should any cairn or grave of a Native American be discovered during site development, work shall temporarily be halted at the specific site and the Sheriff's Office as well as the State Historic Preservation Office of the Department of Conservation and Natural Resources shall be immediately notified per NRS 383.170.

- e. The business license will be obtained to for the new use.
- f. Construction activities shall be limited to the hours between 7am to 7pm, Monday through Saturday only. Any construction machinery activity or any noise associated with the construction activity are also limited to these hours.
- g. Any signage on the parcel shall adhere to the applicable requirements of WCC Article 505, *Sign Regulations*.
- h. Pursuant to WCC Section 110.412.40(d), *Screening Adjoining Residential Uses*, a solid decorative fence of no less than six (6) feet shall be erected along the entire length of the common property line (eastern property line) adjacent to the residential uses. The fence may be constructed of stone, concrete, metal, synthetic or vinyl, but not wood. A note shall be placed on the building plans stating the fence height and material type.
- i. Pursuant to WCC Section 110.414.21(a)(2), exterior light fixtures within one hundred (100) feet of a residential use shall not exceed a height of twelve (12) feet. A note shall be placed on the building plans stating the exterior light fixtures within one hundred (100) feet of a residential use will not exceed a height of twelve (12) feet.
- j. Pursuant to WCC Section 110.414.21(c), non-reflective building materials shall be used for exterior walls and roof surfaces, controlling angles of reflection, and placing landscaping and screening in appropriate locations. Reflective surfaces, such as windows, shall be properly addressed for reflection by such means as tinting or other appropriate

methods. A note shall be placed on the building plans detailing the methods used to address the reflective surfaces such as windows.

- k. The following **Operational Conditions** shall be required for the life of the business:
 - i. This special use permit shall remain in effect until or unless it is revoked or is inactive for one year.
 - ii. Failure to comply with any of the conditions of approval shall render this approval out of conformance and subject to revocation.
 - iii. The applicant and any successors shall direct any potential purchaser/operator of the site to meet with Planning and Building to review conditions of approval prior to the final sale of the site and/or the administrative permit. Any subsequent purchaser/operator of the site and/or the administrative permit shall notify Planning and Building of the name, address, telephone number, and contact person of the new purchaser/operator within 30 days of the final sale.
 - iv. This special use permit shall remain in effect as long as the business is in operation and maintains a valid business license.
- l. Interior lighting, except for limited emergency lighting, shall be turned off at 10:00 p.m. each night in order to reduce nighttime light glare resulting from this project.

Washoe County Engineering and Capital Projects

- 2. The following conditions are requirements of the Engineering Division, which shall be responsible for determining compliance with these conditions.

Contact Name – Robert Wimer, P.E., 775.328.2059, RWimer@washoecounty.gov

- a. A complete set of construction improvement drawings, including an on-site grading plan, shall be submitted when applying for a building/grading permit. Grading shall comply with best management practices (BMP's) and shall include detailed plans for grading, site drainage, erosion control (including BMP locations and installation details), slope stabilization, and mosquito abatement. Placement or removal of any excavated materials shall be indicated on the grading plan. Silts shall be controlled on-site and not allowed onto adjacent property.
- b. Operations and Maintenance (O&M) Manual: The developer shall submit an O&M manual for use by the Landscape Maintenance Association (LMA), Property Owner's Association (POA), or sub association thereof, that identifies ongoing and long-term maintenance of infrastructure items including, but not necessarily limited to, private roadways, graded slopes, private storm drainage infrastructure, landscaping, community amenities, retaining walls, rockery walls, and pedestrian sidewalks or pathways within common areas for review and approval by the County Engineer prior to the approval of any grading or building permit for this project. The O&M manual shall address inspection frequency, storm intensity triggers for inspection and/or repair, types of equipment to be used for the operation and maintenance of the common area improvements, and a site plan that graphically depicts the access points and features that will be owned and maintained by the LMA or POA.
- c. A detailed hydrology/hydraulic report, in conformance with the standards included in the Truckee Meadows Regional Drainage Manual, prepared by a professional engineer licensed in the State of Nevada shall be submitted to the Engineering Division for review and approval. The report shall include the locations, points of entry and discharge, flow rates, and flood limits of all 5- and 100-year storm flows impacting onsite and offsite areas and the methods for handling those flows. The report shall include all storm drain pipe and

ditch sizing calculations, including a discussion of and mitigation measure design for any impacts on existing offsite drainage facilities and properties. Additionally, any increase in storm water runoff resulting from the development and based upon the 5- and 100-year storms shall be detained on site and attenuated to existing flow rates for discharge to the satisfaction of the County Engineer.

- d. Standard reinforced concrete headwalls or other approved alternatives shall be placed on the inlet and outlet of all drainage structures and rip rap shall be used to prevent erosion at the inlets and outlets of all pipe culverts to the satisfaction of the County Engineer.
- e. The following note shall be added to the construction drawings; "All properties, regardless of if they are located within or outside of a FEMA designated flood zone, may be subject to flooding. The property owner is required to maintain all drainage easements and natural drainages and not perform or allow unpermitted and unapproved modifications to the property that may have detrimental impacts to surrounding properties."

Contact Name – Mitchell Fink, P.E., 775.328.2050, MFink@washoecounty.gov

- f. All roadway improvements necessary (including but not limited to, curb, gutter, sidewalk, signing and striping, driveway access, and street lighting) to serve the project shall be designed and constructed to County standards and specifications to the satisfaction of the County Engineer.
- g. The applicant shall provide documentation of access to the site to the satisfaction of the County Engineer. Additionally, prior to approval of a building permit or grading permit, the applicant shall provide documentation from the Regional Transportation Commission (RTC) that the design of project structures, storm drainage infrastructure, and access to the site is in conformance with the Butch Cassidy Drive extension project.
- h. Primary access to the project site shall not be allowed from the Mt. Rose Highway. Secondary emergency access may be allowed from the Mt. Rose Highway. Appropriate safety design shall be included in the plans to accommodate safe crossing of the existing multiuse path and access to the Mt. Rose Highway adjacent to the north property line. The County Engineer shall determine compliance with this condition.
- i. An approved occupancy permit shall be obtained from the Nevada Department of Transportation (NDOT), for access or improvements to, from, or under roads, and highways maintained by NDOT and a copy of said permit provided to the Engineering Division prior to issuance of permit.
- j. The minimum pavement structural section shall be four inches (4") of asphalt over six inches (6") of granular base for local and collector roadways and five inches (5") of asphalt over six inches (6") of granular base for bus routes and arterial roadways to the satisfaction of the County Engineer. All subgrade materials shall meet a minimum R-Value of 30 to the satisfaction of the County Engineer.
- k. The applicant shall submit a detailed geotechnical analysis and report for pavement design recommendations to the County Engineer for review and approval. The report shall be based on the estimated traffic loadings for a 20-year design life and shall include assumptions concerning the distribution of trucks and buses, to include project construction truck traffic. The resultant pavement thickness in the geotechnical analysis shall be used if the report indicates a structural section that exceeds the minimum County standards, if required.
- l. Prior to ground-disturbing activity, a proposed Construction Traffic Haul Route Plan shall be submitted to the Engineering Division for review and approval. Any existing or proposed roads that will be used as construction haul routes and are not designated truck routes

shall be evaluated by a geotechnical study to determine the existing structural section and its load carrying capacity. If the pavement section is inadequate to support the proposed construction loading, the roadway shall be redesigned or reconstructed as needed to provide a 20-year design life in accordance with the AASHTO Interim Guide for Flexible Pavement.

- m. A safe walkway route, including any required crossings, shall be provided for all pedestrians. A pedestrian walkway plan shall be approved by the County Engineer prior to the issuance of the building permit.
- n. The applicant shall provide temporary traffic control plans for review and approval by the County Engineer prior to commencement of construction.
- o. The applicant shall submit an encroachment and excavation permit application for review and approval for any construction within Washoe County Right of Way.

Contact Name – Katrina Pascual, P.E., 775.954.4648, KPascual@washoecounty.gov

- p. All necessary utilities shall be stubbed beyond the edge of pavement for future development to the satisfaction of the County Engineer.
- q. The applicant shall conform to all conditions imposed by intergovernmental agreements required to provide sewer and reclaim water service to the subject project, and, if required, be a party to any such agreements.
- r. The applicant shall conform to all Washoe County utility design standards, including but not limited to, gravity sewer collection system, lift station design, and reclaim water design.

Washoe County Water Management Planner Coordinator

- 3. The following conditions are requirements of Washoe County Water Management Planner Coordinator, who shall be responsible for determining compliance with these conditions.

Contact: Contact: Timber Weiss, PE, 775.954.4626, tweiss@washoecounty.gov

- a. A TMWA discovery process must be completed prior to this parcel being annexed into TMWA's service territory. The applicant shall conform with the requirements of TMWA in regard to water rights and water service. TMWA requires that all water service conditions to be met prior to approval of building permits. Recommend approval of this permit, with written approval or will-serve from TMWA to provide service for the additional use on this property prior to building permit issuance.

Washoe County Regional Parks and Open Space

- 4. The following conditions are requirements of the Engineering Division, which shall be responsible for determining compliance with these conditions.

Contact Name – Faye-Marie Pekar, Park Planner, 775.328.6100, FPekar@washoecounty.gov

- a. Should any earthen materials imported to the site shall be "certified weed free" to prevent the spread of noxious and invasive weeds.
- b. The project shall comply with Washoe County Code Section 110.412.67, Revegetation.
- c. All undeveloped areas disturbed as a result of project activities shall be revegetated utilizing a native seed mix as reviewed and approved by the Washoe Storey Conservation District and/or Washoe County Regional Parks and Open Space.

- d. In alignment with the Washoe County Open Space and Natural Resource Management Plan, Recreational Resources Goal 6: Minimize resource pressures posed by development near open space areas and Goal 6.3, fire breaks between developed areas and open space should be provided by the applicant to minimize wildland fire danger and minimize other potential urban interface conflicts.

Truckee Meadows Fire Protection District

5. The following condition is a requirement of the Truckee Meadows Fire Protection District, which shall be responsible for determining compliance with this condition.

Contact Name – Brittany Lemon, Fire Captain, 775.326.6079, blemon@tmfpd.us

- a. This project shall meet and comply with all requirements of currently adopted TMFPD fire codes, ordinances, and standards at the time of construction to include infrastructure for fire apparatus access roads and water supply. <https://tmfpd.us/fire-code/>

Washoe County Health District

6. The following condition is a requirement of the Health District, which shall be responsible for determining compliance with this condition. The District Board of Health has jurisdiction over all public health matters in the Health District. Any conditions set by the Health District must be appealed to the District Board of Health.

Contact Name – James English, EHS Supervisor, 775.328.2434, JEnglish@washoecounty.gov

- a. If the special use permit is approved, all subsequent building permits and plan reviews must be routed to the WCHD for review and approval prior to permit issuance or construction.

Nevada Division of Water Resources

7. The following condition is a requirement of the Nevada Division of Water Resources, which shall be responsible for determining compliance with these conditions.

Contact Name – Steve Shell, Water Rights Specialist, 775.684.2836, sshell@water.nv.gov

- a. The subject property lies within the Truckee Meadows Water Authority service area. Municipal water service is subject to Truckee Meadows Water Authority rules and regulations and approval by the Office of the State Engineer regarding water quantity and availability. A Will Serve from Truckee Meadows Water Authority and a mylar map of the proposed project must be presented to the State Engineer for approval and signed through his office prior to development.

Nevada Department of Transportation (NDOT)

8. The following condition is a requirement of the Nevada Department of Transportation, which shall be responsible for determining compliance with these conditions.

Contact Name – Jeff Graham, Engineering Services Manager, 775.834.8382, jeffrey.graham@dot.nv.gov

- a. NDOT requires the use of permitted access to the state highway system. A NDOT occupancy permit will be required for the proposed improvements within and adjacent to SR 431 right of way. The maintaining agency of the access will be required to be the permittee.

- b. All work proposed within or adjacent to the SR 431 right of way will require an encroachment permit and must comply with NDOT's Standard Plans, Access Management System and Standards, Terms and Conditions Relating to Right-of-Way Occupancy Permits, and the Drainage Manual current version at the time of application. Please contact the NDOT District II Permits Office at (775) 834-8330 for information about obtaining NDOT occupancy permits.
- c. Since the site is located directly adjacent to SR 431 and has the potential to effect area drainage patterns, the applicant should be required to obtain an occupancy permit from NDOT for the drainage encroachment.

Regional Transportation Commission (RTC)

- 9. The following condition is a requirement of RTC, which shall be responsible for determining compliance with these conditions.

Contact Name – Kimberly Diegle, P.E., 775.348.0400, kdiegle@rtcwashoe.com

- a. RTC requests continued coordination between the two projects. RTC has no objection to the general location of the connection to Butch Cassidy Drive. Further design coordination for items including but not limited to pedestrian access, placement of permanent structures, and drainage improvements will be needed and RTC further anticipates participation in review of building and grading permits for this project.
- b. Consideration needs to be made for the existing multi-use trail, drainage, and barrier rail along Mt. Rose Highway that the emergency access will be crossing. ADA ramps and detectable warning tiles are anticipated to be required at the driveway crossing, driveway grading will need to meet ADA requirements, and appropriate protection/crash attenuation that meets NDOT requirements needs to be addressed for any gap in the barrier rail as a part of the NDOT encroachment permit.

*** End of Conditions ***

From: [Steve Shell](#)
To: [Evans, Timothy](#)
Subject: WSUP23-0027
Date: Tuesday, August 15, 2023 4:37:25 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.jpg](#)
[image006.png](#)
[image008.png](#)
[image010.jpg](#)
[image011.jpg](#)
[image012.jpg](#)

[**NOTICE:** This message originated outside of Washoe County -- **DO NOT CLICK** on **links** or open **attachments** unless you are sure the content is safe.]

WSUP23-0027 (Supreme Storage):

The subject property lies within the Truckee Meadows Water Authority service area. Municipal water service is subject to Truckee Meadows Water Authority rules and regulations and approval by the Office of the State Engineer regarding water quantity and availability. A Will Serve from Truckee Meadows Water Authority and a mylar map of the proposed project must be presented to the State Engineer for approval and signed through his office prior to development.

As of June 1, 2021, the Office of the State Engineer is open to the public. Please call 684-2800 upon arrival and a representative will come down to escort you to our office.

Steve Shell
Water Rights Specialist II
Department of Conservation and Natural Resources
Nevada Division of Water Resources
901 S. Stewart St., Suite 2002
Carson City, NV 89701
sshell@water.nv.gov
(O) 775-684-2836 | (F) 775-684-2811

From: [COOPER, CLIFFORD E](#)
To: [Evans, Timothy](#)
Subject: RE: WSUP23-0027 (Supreme Storage)
Date: Wednesday, August 16, 2023 6:51:16 AM
Attachments: [image001.png](#)

[**NOTICE:** This message originated outside of Washoe County -- **DO NOT CLICK** on **links** or open **attachments** unless you are sure the content is safe.]

Tim,

AT&T Nevada has facilities in the NDOT ROW that is SR431 (Mt Rose Hwy). The proposed landscaping that fronts the roadway may conflict with the 3-duct structure that would be buried 36-43" deep in the vicinity. The conduit is represented by the magenta line shown below. USA/811 would allow the facilities to be located for the developer/contractor's information.

Cliff Cooper
MGR OSP PLANNING
AT&T NEVADA
1375 Capital Blvd rm 115
Reno, NV 89502
ROW Office: 775-453-7578
Email: cc2132@att.com
TEXTING and DRIVING...It Can Wait

Evans, Timothy

From: Lemon, Brittany
Sent: Thursday, August 17, 2023 2:26 PM
To: Evans, Timothy
Cc: Way, Dale
Subject: WSUP23-0027 (Supreme Storage) Conditions of Approval

Hi Tim,

“This project shall meet and comply with all requirements of currently adopted TMFPD fire codes, ordinances, and standards at the time of construction to include infrastructure for fire apparatus access roads and water supply.”

<https://tmfpd.us/fire-code/>.

Thank you,

Brittany Lemon

Fire Captain - Fire Prevention | Truckee Meadows Fire & Rescue

blemon@tmfpd.us | Office: 775.326.6079 | Cell: 775.379.0584

3663 Barron Way, Reno, NV 89511



"Committed to excellence, service, and the protection of life and property in our community"



Date: August 24, 2023

To: Tim Evans, Planner

From: Timber Weiss, P.E., Licensed Engineer

Re: Special Use Permit Case Number WSUP23-0027 (Supreme Storage)
APN 049-402-01

GENERAL PROJECT DISCUSSION

For hearing, discussion, and possible action to approve a special use permit for grading and the construction of a 583-unit storage facility, and an onsite office space, for personal storage comprised of two (2) structures totaling 75,000 square feet. Proposed ground disturbance to construct the facility is 2.55 acres, with approximately 6,871 cubic yards of cut, 7,039 cubic yards of fill, and 168 cubic yards of imported material.

The Community Services Department (CSD) recommends approval of this project with the following Water Rights conditions:

A TMWA discovery process must be completed prior to this parcel being annexed into TMWA's service territory. The applicant shall conform with the requirements of TMWA in regard to water rights and water service. TMWA requires that all water service conditions to be met prior to approval of building permits. Recommend approval of this permit, with written approval or will-serve from TMWA to provide service for the additional use on this property prior to building permit issuance.



JOE LOMBARDO
Governor

STATE OF NEVADA
DEPARTMENT OF TRANSPORTATION
310 Galletti Way
Sparks, Nevada 89431

TRACY LARKIN THOMASON, P.E.
Director

August 24, 2023

Washoe County Planning Division
1001 E. 9th St,
Reno, NV 89512
Attention: Tim Evans – Planner

SENT VIA ELECTRONIC MAIL

RE: Supreme Storage - **Case Number WSUP23-0027**

Dear Mr. Evans,

Nevada Department of Transportation (NDOT) District II staff has reviewed the application received via e-mail on August 16th, 2023 and provides comments accordingly.

Supreme Storage - For hearing, discussion, and possible action to approve a special use permit for grading and the construction of a 583-unit storage facility, and an onsite office space, for personal storage comprised of two (2) structures totaling 75,000 square feet. Proposed ground disturbance to construct the facility is 2.55 acres, with approximately 6,871 cubic yards of cut, 7,039 cubic yards of fill, and 168 cubic yards of imported material.

NDOT comments:

1. The project is directly adjacent to Mt Rose Highway which is an NDOT maintained road that is officially designated as SR 431 and functionally classified as a Minor Arterial.
2. The project proposes access to SR 431. A trip generation letter with trip distribution for the development will be required for determination of possible mitigations and if a traffic impact study per NDOT's Terms and Conditions Relating to Right-of-Way Occupancy Permits will be required.
3. NDOT requires the use of permitted access to the state highway system. A NDOT occupancy permit will be required for the proposed improvements within and adjacent to SR 431 right of way. The maintaining agency of the access will be required to be the permittee.
4. All work proposed within or adjacent to the SR 431 right of way will require an encroachment permit and must comply with NDOT's Standard Plans, Access Management System and Standards, Terms and Conditions Relating to Right-of-Way Occupancy Permits, and the Drainage Manual current version at the time of application. Please contact

the NDOT District II Permits Office at (775) 834-8330 for information about obtaining NDOT occupancy permits.

5. Since the site is located directly adjacent to SR 431 and has the potential to effect area drainage patterns, the applicant should be required to obtain an occupancy permit from NDOT for the drainage encroachment.
6. This letter does not provide for approval or disapproval of any improvements proposed by the project. NDOT review during the occupancy permit process may result in modification to the proposed improvements or denial.
7. The State defers to municipal government for land use development decisions. Public involvement for community development related improvements within NDOT right of way should be considered during the municipal land use development process. Significant improvements proposed within NDOT right of way may require additional public involvement. It is the responsibility of the applicant to perform such additional public involvement.

Thank you for the opportunity to review this application. NDOT reserves the right to incorporate further changes and/or comments as these applications and design reviews progress. Should you have any questions, please contact Jeff Graham at (775) 834-8382.

Sincerely,

DocuSigned by:



EEF9C06ADD034C7...

Jeff Freeman, PE

Engineering Services Manager

District II

JF:ms

Cc: Bhupinder Sandhu – Acting DII District Engineer
Jeff Graham – Traffic Engineer
District II Traffic Engineering Distribution List
Washoe County Planning Division
File

**WASHOE COUNTY
HEALTH DISTRICT**
ENHANCING QUALITY OF LIFE

August 25, 2023

Washoe County Community Services
Planning and Development Division

RE: Supreme Storage; 049-402-01
Special Use Permit; SUP23-0027

Dear Washoe County Staff:

The following conditions are requirements of the Washoe County Health District, Environmental Health Services Division, (WCHD), which shall be responsible for determining compliance with these conditions.

Contact Name – James English - jenglish@washoecounty.us

- a) Condition #1: The WCHD has reviewed this application and has no objections to the approval of the special use permit as submitted.
- b) Condition #2: If the special use permit is approved, all subsequent building permits and plan reviews must be routed to the WCHD for review and approval prior to permit issuance or construction.

If you have any questions or would like clarification regarding the foregoing, please contact James English, EHS Supervisor at jenglish@washoecounty.us regarding all Health District comments.

Sincerely,



James English, REHS, CP-FS
EHS Supervisor
Environmental Health Services
Washoe County Health District

From: [Evans, Timothy](#)
To: [Graham, Jeffrey](#)
Subject: RE: Special Use Permit WSUP23-0027; 3900 Mount Rose Highway, Reno; APN 049-402-01
Date: Tuesday, August 29, 2023 10:14:00 AM
Attachments: [image002.png](#)
[image003.png](#)

Hi Jeff,

Thank you for the information and clarification.

Best,



Tim Evans
Planner, Planning & Building Division | Community Services Department
CSD Main Phone: 775-328-3600 Direct Line: 775-328-2314
Visit us first online: www.washoecounty.gov/csd

From: Graham, Jeffrey <jeffrey.graham@dot.nv.gov>
Sent: Tuesday, August 29, 2023 10:14 AM
To: Evans, Timothy <TEvans@washoecounty.gov>
Subject: RE: Special Use Permit WSUP23-0027; 3900 Mount Rose Highway, Reno; APN 049-402-01

[NOTICE: This message originated outside of Washoe County -- DO NOT CLICK on links or open attachments unless you are sure the content is safe.]

Hi Tim,

The portion of the comment referring to the impact study should have been omitted. A traffic impact study will not be required. Please use the guidance provided in the email.

Thank you,



Jeff Graham
Traffic Engineer
District II
Nevada Department of Transportation
O 775.834.8382 M 775.443.7462
e jeffrey.graham@dot.nv.gov | w dot.nv.gov

From: Evans, Timothy <TEvans@washoecounty.gov>

Sent: Tuesday, August 29, 2023 9:45 AM

To: Hawvichorst, Michelle <mhawvichorst@dot.nv.gov>; Graham, Jeffrey <jeffrey.graham@dot.nv.gov>

Subject: RE: Special Use Permit WSUP23-0027; 3900 Mount Rose Highway, Reno; APN 049-402-01

Hi Jeff,

I realized the email I sent went to an incorrect email for you.

See my email below. In addition to the email I sent below, the project applicant provided me with an email from you stating that a traffic study wouldn't be required. I was hoping you could provide some clarification as the comment letter I received on 8/24 requested a trip generation letter to determine if a traffic study would be required.

Thanks,



Tim Evans

Planner, Planning & Building Division | Community Services Department

CSD Main Phone: 775-328-3600 Direct Line: 775-328-2314

Visit us first online: www.washoecounty.gov/csd

From: Evans, Timothy

Sent: Monday, August 28, 2023 12:04 PM

To: mgriffin@dot.nv.gov; jgraham@dot.nv.gov

Subject: Special Use Permit WSUP23-0027; 3900 Mount Rose Highway, Reno; APN 049-402-01

Good Afternoon,

Planning staff received comments from the Nevada Department of Transportation (NDOT) for the 583-unit personal storage facility located at 3900 Mount Rose Highway, Reno; APN 049-402-01. Comment #2 of the comment letter (attached) received from NDOT on August 24, 2023, stated the following:

"The project proposes access to SR 431. A trip generation letter with trip distribution for the development will be required for determination of possible mitigations and if a traffic impact study per NDOT's Terms and Conditions Relating to Right-of-Way Occupancy Permits will be required."

Washoe County Planning staff recently received a Trip Generation letter and study, which the applicant initially submitted with the special use permit application and resubmitted, for the Supreme Storage facility located at 3900 Mount Rose Highway, Reno; APN 049-402-01.

Please review the attached trip generation letter and study from the applicant to address Comment #2. Please provide any comments and/or conditions for the document on or prior to September 11, 2023.

Best Regards,



Tim Evans

Planner, Planning & Building Division | Community Services Department

CSD Main Phone: 775-328-3600 Direct Line: 775-328-2314

Visit us first online: www.washoecounty.gov/csd



WASHOE COUNTY
COMMUNITY SERVICES DEPARTMENT
Regional Parks and Open Space

1001 EAST 9TH STREET
 RENO, NEVADA 89520-0027
 PHONE (775) 328-3600
 FAX (775) 328.3699

TO: Tim Evans, Planner

FROM: Faye-Marie Pekar, Park Planner

DATE: August 29, 2023

SUBJECT: Special Use Permit Case Number WSUP23-0027 (Supreme Storage)



I have reviewed the application for case number WSUP23-0027 on behalf of the Washoe County Regional Parks and Open Space Program (Parks Program) and prepared the following comments:

If approved, this special use permit would allow for grading and construction of a 583 unit storage facility, and an onsite office space, for personal storage comprised of two structures totaling 75,000 square feet. The proposal includes 2.55 acres of ground disturbance with ±6,871 cubic yards of cut, ±7,039 cubic yards of fill, and ±168 cubic yards of imported material. The subject parcel is located adjacent to Washoe County Galena Terrace Open Space at its southern border and adjacent to a public parcel administered by the United States Forest Service. Given these considerations, the Parks Program requires the following conditions of approval:

1. Should any earthen materials imported to the site shall be “certified weed free” to prevent the spread of noxious and invasive weeds.
2. The project shall comply with Washoe County Code Section 110.412.67, Revegetation.
3. All undeveloped areas disturbed as a result of project activities shall be revegetated utilizing a native seed mix as reviewed and approved by the Washoe Storey Conservation District and/or Washoe County Regional Parks and Open Space.
4. In alignment with the Washoe County Open Space and Natural Resource Management Plan, Recreational Resources Goal 6: Minimize resource pressures posed by development near open space areas and Goal 6.3, fire breaks between developed areas and open space should be provided by the applicant to minimize wildland fire danger and minimize other potential urban interface conflicts.



INTEGRITY



EFFECTIVE COMMUNICATION



QUALITY PUBLIC SERVICE

WWW.WASHOECOUNTY.US



Date: May 7, 2024

To: Tim Evans, Planner

From: Janelle K. Thomas, P.E., C.F.M., Senior Licensed Engineer
Robert Wimer, P.E., Licensed Engineer
Mitchell Fink, P.E., Licensed Engineer

Re: Special Use Permit for **Supreme Storage WSUP23-0027**
APN 049-402-01

Washoe County Engineering staff has reviewed the above referenced application. The Special Use Permit is for the construction of a self-storage facility and is located on approximately 5.026 acres at approximately 500 feet east of the intersection of Mount Rose Highway and Thomas Creek Road. The parcel number includes the following: 049-402-01. The Engineering and Capital Projects Division recommends approval with the following comments and conditions of approval which supplement applicable County Code and are based upon our review of the site and the application prepared by KLS Planning & Design. The County Engineer shall determine compliance with the following conditions of approval.

For questions related to sections below, please contact the staff's name referenced.

GENERAL CONDITIONS

Contact Information: Robert Wimer, P.E. (775) 328-2059

Conditions:

1. A complete set of construction improvement drawings, including an on-site grading plan, shall be submitted when applying for a building/grading permit. Grading shall comply with best management practices (BMP's) and shall include detailed plans for grading, site drainage, erosion control (including BMP locations and installation details), slope stabilization, and mosquito abatement. Placement or removal of any excavated materials shall be indicated on the grading plan. Silts shall be controlled on-site and not allowed onto adjacent property.
2. Operations and Maintenance (O&M) Manual: The developer shall submit an O&M manual for use by the Landscape Maintenance Association (LMA), Property Owner's Association (POA), or sub association thereof, that identifies ongoing and long-term maintenance of infrastructure items including, but not necessarily limited to, private roadways, graded slopes, private storm drainage infrastructure, landscaping, community amenities, retaining walls, rockery walls, and pedestrian sidewalks or pathways within common areas for review and approval by the County Engineer prior to the approval of any grading or building permit for this project. The O&M manual shall address inspection frequency, storm intensity triggers for inspection and/or repair, types of equipment to be used for the operation and maintenance of the common area improvements, and a site plan that graphically depicts the access points and features that will be owned and maintained by the LMA or POA.

DRAINAGE (COUNTY CODE 110.416, 110.420, and 110.421)

Contact Information: Robert Wimer, P.E. (775) 328-2059

Conditions:

1. A detailed hydrology/hydraulic report, in conformance with the standards included in the Truckee Meadows Regional Drainage Manual, prepared by a professional engineer licensed in the State of Nevada shall be submitted to the Engineering Division for review and approval. The report shall include the locations, points of entry and discharge, flow rates, and flood limits of all 5- and 100-year storm flows impacting onsite and offsite areas and the methods for handling those flows. The report shall include all storm drain pipe and ditch sizing calculations, including a discussion of and mitigation measure design for any impacts on existing offsite drainage facilities and properties. Additionally, any increase in storm water runoff resulting from the development and based upon the 5- and 100-year storms shall be detained on site and attenuated to existing flow rates for discharge to the satisfaction of the County Engineer.
2. Standard reinforced concrete headwalls or other approved alternatives shall be placed on the inlet and outlet of all drainage structures and rip rap shall be used to prevent erosion at the inlets and outlets of all pipe culverts to the satisfaction of the County Engineer.
3. The following note shall be added to the construction drawings; "All properties, regardless of if they are located within or outside of a FEMA designated flood zone, may be subject to flooding. The property owner is required to maintain all drainage easements and natural drainages and not perform or allow unpermitted and unapproved modifications to the property that may have detrimental impacts to surrounding properties."

TRAFFIC AND ROADWAY (COUNTY CODE 110.436)

Contact Information: Mitchell Fink, P.E. (775) 328-2050

Discussion:

While Washoe County Engineering does not support direct access onto Mt. Rose Highway from this proposed project due to the additional direct access causing degradation of traffic safety on the Mt. Rose highway corridor, the proposed secondary gated emergency access is acceptable if the civil design can integrate safe access across the existing multiuse path and barrier rail along the project frontage to Mt. Rose Highway. The applicant shall be required to provide documentation of an approved NDOT encroachment permit for any improvements in NDOT right of way prior to issuance of any site improvement or building permit on the project site.

Forest Area Plan policy F 5.8 indicates a need to analyze the extension of Butch Cassidy Drive to the existing intersection of Thomas Creek Road. Washoe County Engineering shall require coordination and approval from the Regional Transportation Commission (RTC) for integration of the Butch Cassidy Drive extension plans to eliminate conflicts with site improvements, including construction of structures, storm drainage improvements, and access roadways.

Conditions:

1. All roadway improvements necessary (including but not limited to, curb, gutter, sidewalk, signing and striping, driveway access, and street lighting) to serve the project shall be designed and constructed to County standards and specifications to the satisfaction of the County Engineer.

2. The applicant shall provide documentation of access to the site to the satisfaction of the County Engineer. Additionally, prior to approval of a building permit or grading permit, the applicant shall provide documentation from the Regional Transportation Commission (RTC) that the design of project structures, storm drainage infrastructure, and access to the site is in conformance with the Butch Cassidy Drive extension project.
3. Primary access to the project site shall not be allowed from the Mt. Rose Highway. Secondary emergency access may be allowed from the Mt. Rose Highway. Appropriate safety design shall be included in the plans to accommodate safe crossing of the existing multiuse path and access to the Mt. Rose Highway adjacent to the north property line. The County Engineer shall determine compliance with this condition.
4. An approved occupancy permit shall be obtained from the Nevada Department of Transportation (NDOT), for access or improvements to, from, or under roads, and highways maintained by NDOT and a copy of said permit provided to the Engineering Division prior to issuance of permit.
5. The minimum pavement structural section shall be four inches (4") of asphalt over six inches (6") of granular base for local and collector roadways and five inches (5") of asphalt over six inches (6") of granular base for bus routes and arterial roadways to the satisfaction of the County Engineer. All subgrade materials shall meet a minimum R-Value of 30 to the satisfaction of the County Engineer.
6. The applicant shall submit a detailed geotechnical analysis and report for pavement design recommendations to the County Engineer for review and approval. The report shall be based on the estimated traffic loadings for a 20-year design life and shall include assumptions concerning the distribution of trucks and buses, to include project construction truck traffic. The resultant pavement thickness in the geotechnical analysis shall be used if the report indicates a structural section that exceeds the minimum County standards, if required.
7. Prior to ground-disturbing activity, a proposed Construction Traffic Haul Route Plan shall be submitted to the Engineering Division for review and approval. Any existing or proposed roads that will be used as construction haul routes and are not designated truck routes shall be evaluated by a geotechnical study to determine the existing structural section and its load carrying capacity. If the pavement section is inadequate to support the proposed construction loading, the roadway shall be redesigned or reconstructed as needed to provide a 20-year design life in accordance with the AASHTO Interim Guide for Flexible Pavement.
8. A safe walkway route, including any required crossings, shall be provided for all pedestrians. A pedestrian walkway plan shall be approved by the County Engineer prior to the issuance of the building permit.
9. The applicant shall provide temporary traffic control plans for review and approval by the County Engineer prior to commencement of construction.
10. The applicant shall submit an encroachment and excavation permit application for review and approval for any construction within Washoe County Right of Way.

UTILITIES (County Code 422 & Sewer Ordinance)

Contact Information: Katrina Pascual, P.E. (775) 954-4648

Conditions:

1. All necessary utilities shall be stubbed beyond the edge of pavement for future development to the satisfaction of the County Engineer.
2. The applicant shall conform to all conditions imposed by intergovernmental agreements required to provide sewer and reclaim water service to the subject project, and, if required, be a party to any such agreements.
3. The applicant shall conform to all Washoe County utility design standards, including but not limited to, gravity sewer collection system, lift station design, and reclaim water design.



REGIONAL TRANSPORTATION COMMISSION

Metropolitan Planning • Public Transportation & Operations • Engineering & Construction

Metropolitan Planning Organization of Washoe County, Nevada

Tim Evans, Planner

Washoe County Community Services

Subject: Special Use Permit for Supreme Storage WSUP23-0027

Dear Tim,

RTC has reviewed the above reference application and has the following comments:

- The RTC Butch Cassidy Extension Project is identified in the 2050 Regional Transportation Plan (RTP), the Statewide Transportation Improvement Program (STIP), and the RTC/Washoe County Interlocal Cooperative Agreement. This project plans to extend Butch Cassidy Drive from the west end of existing Butch Cassidy to Thomas Creek/Mt. Rose intersection. The project is currently in preliminary design and the alignment for the future extension is not yet finalized. The project is programmed as being complete by the 2030 model year. For more information on the RTP Project, the developer can contact Xuan Wang, RTC Planning Manager at (775) 332-9521 or Kim Diegle RTC Project Manager at (775) 335-1844.
- RTC requests continued coordination between the two projects. RTC has no objection to the general location of the connection to Butch Cassidy Drive. Further design coordination for items including but not limited to pedestrian access, placement of permanent structures, and drainage improvements will be needed and RTC further anticipates participation in review of building and grading permits for this project.
- Consideration needs to be made for the existing multi-use trail, drainage, and barrier rail along Mt. Rose Highway that the emergency access will be crossing. ADA ramps and detectable warning tiles are anticipated to be required at the driveway crossing, driveway grading will need to meet ADA requirements, and appropriate protection/crash attenuation that meets NDOT requirements needs to be addressed for any gap in the barrier rail as a part of the NDOT encroachment permit.

Please let me know if you have any questions.

Thank you,

Kimberly Diegle, P.E.

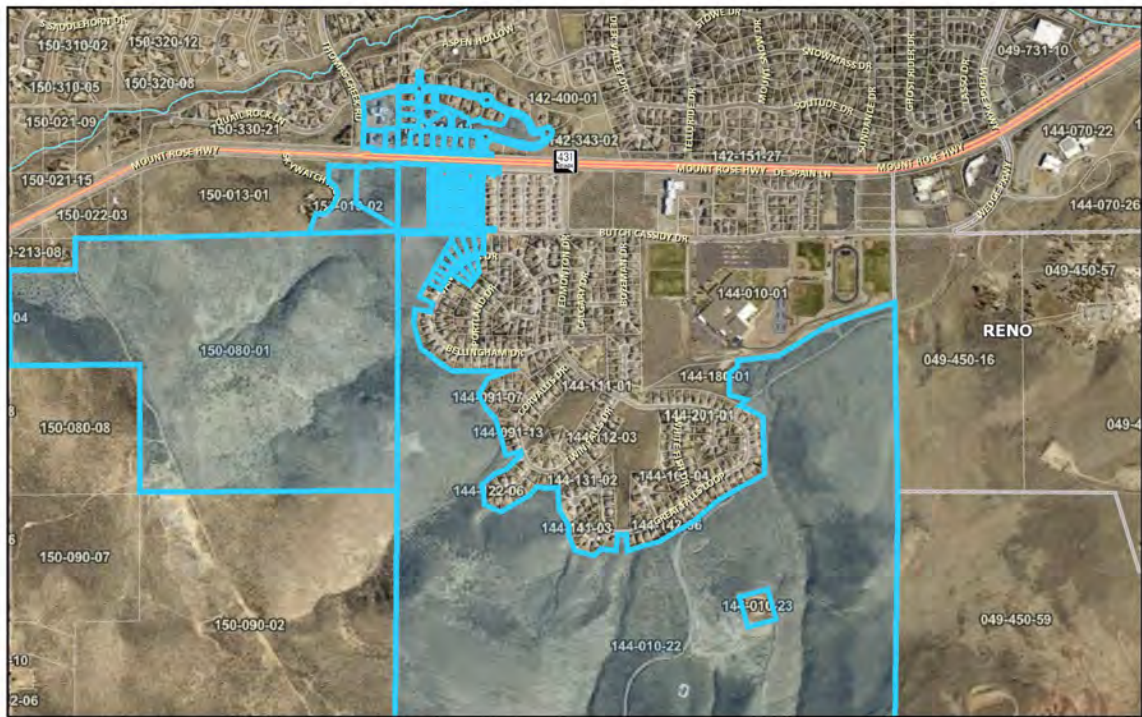
Project Manager

Cc: Jeff Wilbrecht, RTC Engineering Manager
Alex Wolfson, RTC Project Manager
Marquis Williams, RTC Planning
Janelle Thomas, Washoe County Senior Licensed Engineer, Engineering

Public Notice

Washoe County Code requires that public notification for a special use permit must be mailed to a minimum of 30 separate property owners within a minimum 500-foot radius of the subject property a minimum of 10 days prior to the public hearing date. A notice setting forth the time, place, purpose of hearing, a description of the request and the land involved was sent within a 500-foot radius of the subject property. A total of 77 separate property owners were noticed a minimum of 10 days prior to the public hearing date.

Noticing - 77 Parcels within 500 Feet



August 10, 2023

Washoe County GIS
Source: Esri, Maxar, Earthstar Geographics, and the GIS User Community

This information for illustrative purposes only. Not to be used for boundary resolution or location and not intended to be used for measurement, calculation, or delineation.
Washoe County Technology Services - Regional Services Division, 1001 E. 9th St, Building C-200, Reno, NV 89512 www.washoecounty.org/gis (775) 326-2345

Public Notice Map

Special Use Permit Case Number WSUP23-0027

Supreme Storage

Application to Washoe County for a:

Special Use Permit

Prepared by:



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Kimley»Horn

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Prepared for:

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Managing Member
CPD Mt Rose LLC
(206) 910-8282

March 21, 2024

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Appendix A

Application Materials

Washoe County Development Application
Special Use Permit – Supplemental Information

Civil & Landscape Plan Set (full size sheets – 24” x 36”)

Preliminary Landscape Plan
Site and Utility Plan
Grading and Drainage Sheet
Preliminary Sections
Truck Turn Exhibit
Site Photometric Plan
Site Plan
Fire Access Plan

Exterior Site Elevations
Exterior Elevations – Building A
Exterior Elevations – Building B

Project Request

This application contains a request for a **Special Use Permit** for Personal Storage in the NC zone, as well as hillside development and major grading meeting the thresholds defined in Washoe County Development Code Section 110.424.05(a) and 110.438.35(a).

Project Location

The site is composed of one parcel (APN: 049-402-01) totaling 5.03 acres. It is located on Mount Rose Highway near the intersection with Thomas Creek Road, about 1.6 miles west of I-580. The site is currently vacant. Current primary access to the site is via Mount Rose Highway.

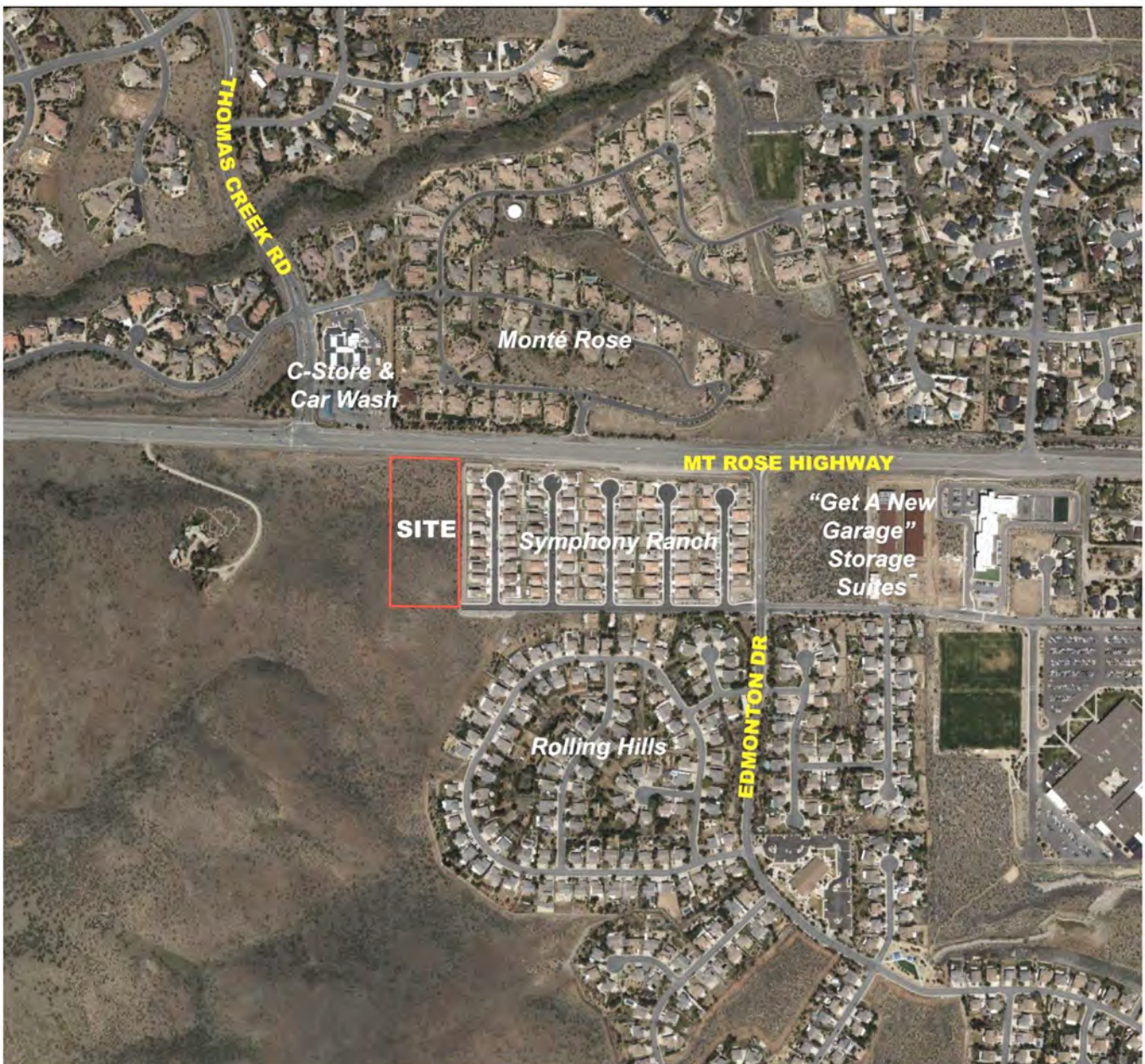


Figure 1 – Vicinity Map

Land Use and Zoning

The site is adjacent to vacant land to the west and south, and residential to the east and north. Commercial (C) was established for the site's land use in the Washoe County Master Plan, and regulatory zoning is Neighborhood Commercial (NC) (See Figures 3 and 4 below). The site is part of the Forest Area Plan within its Mount Rose Scenic Highway Commercial Overlay District (MRSHCOD).

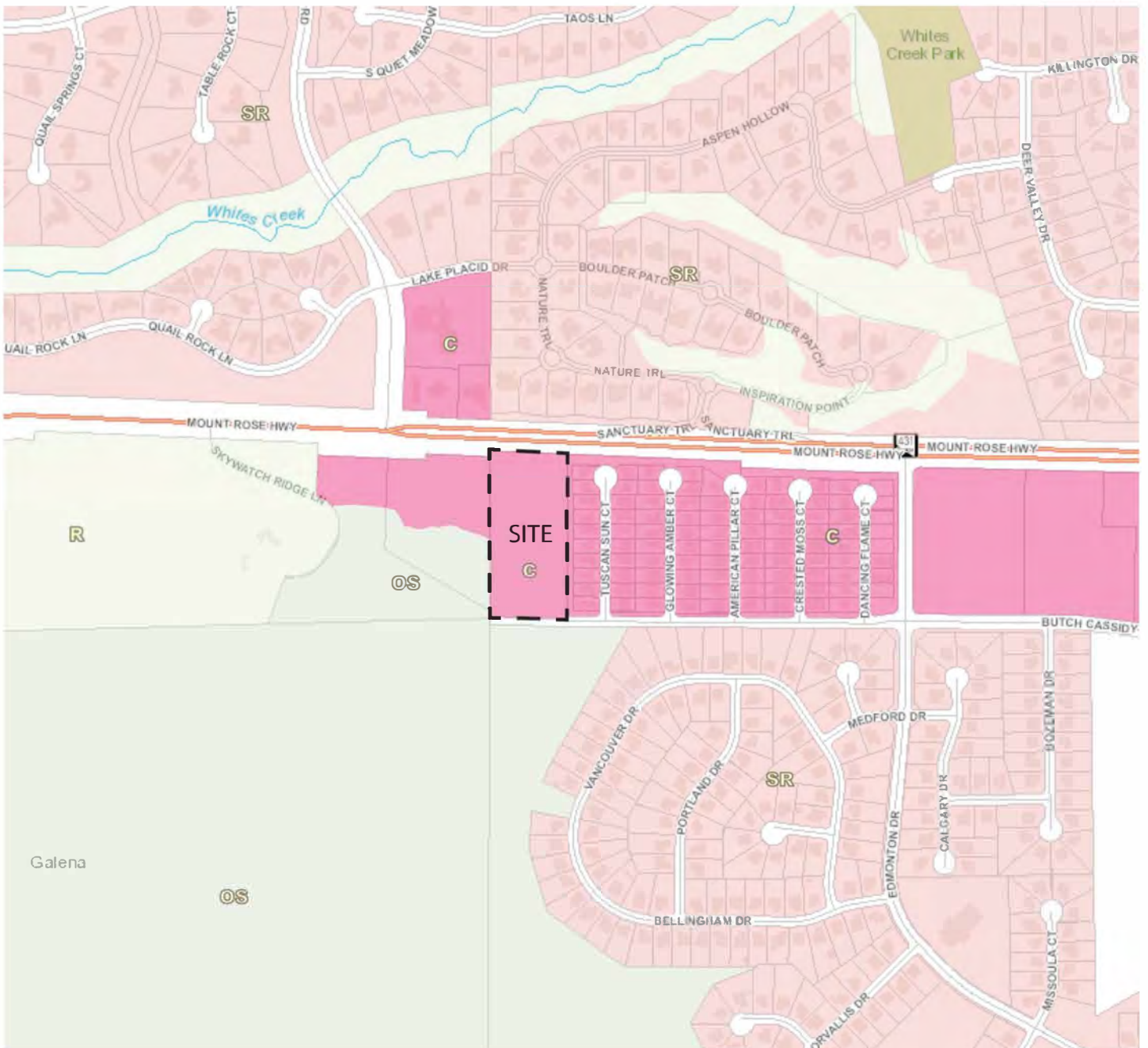


Figure 2 – Washoe County Master Plan

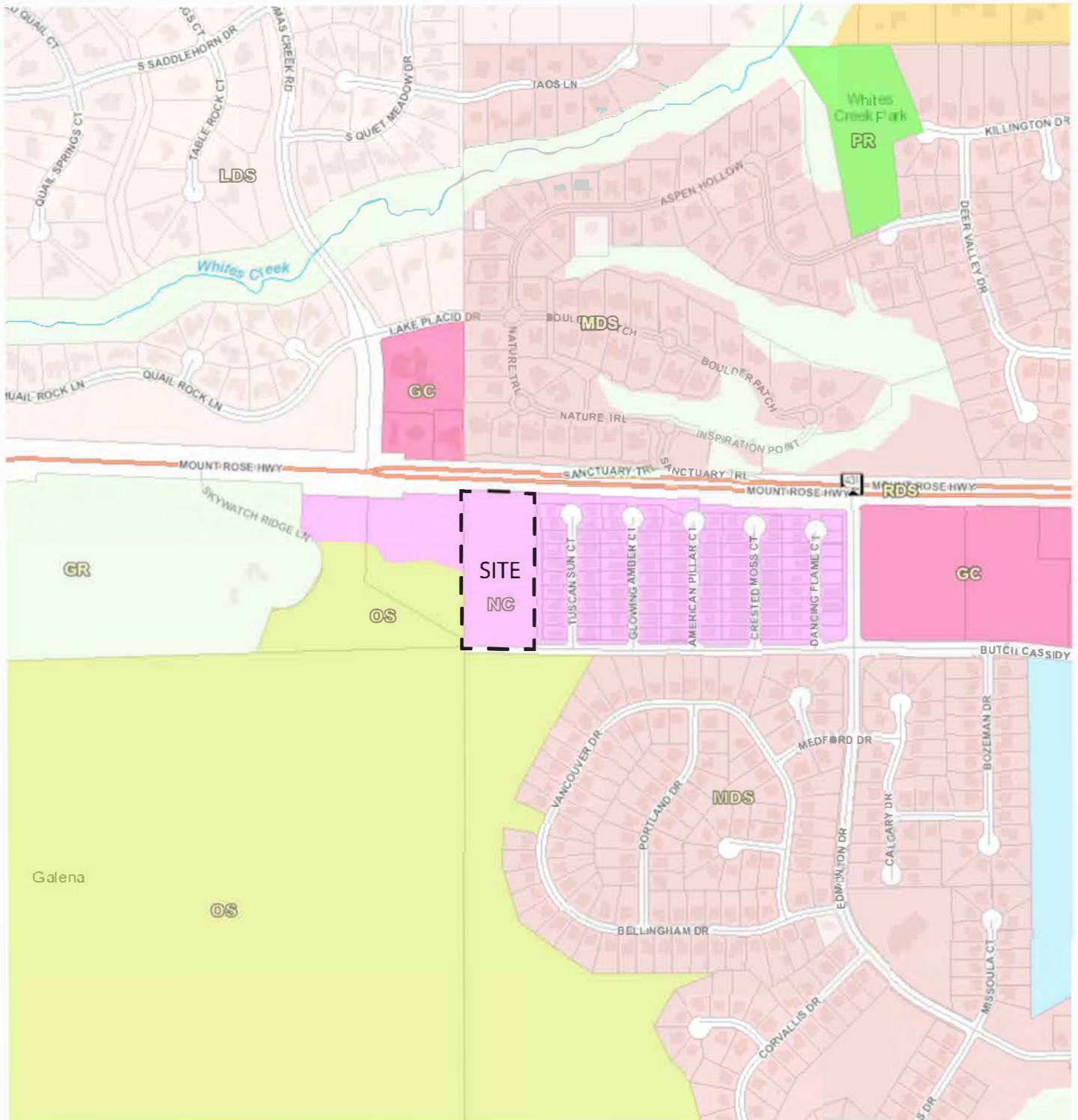


Figure 3 – Washoe County Zoning Map

Project Description

The project proposes a climate control mini-storage building which consists of two buildings. Building A is 2 stories that are 32,125 SF each for a total of 64,250 SF and Building B that is 12,300 SF, for a site total of 76,550 SF. See Figure 4 which is the floor plan and unit mix for Building A. There are 7 parking spaces provided and 139 average daily trips, 9 AM peak hour trips, and 13 PM peak hour trips (see Trip Generation Letter and Parking Analysis in Appendix B).

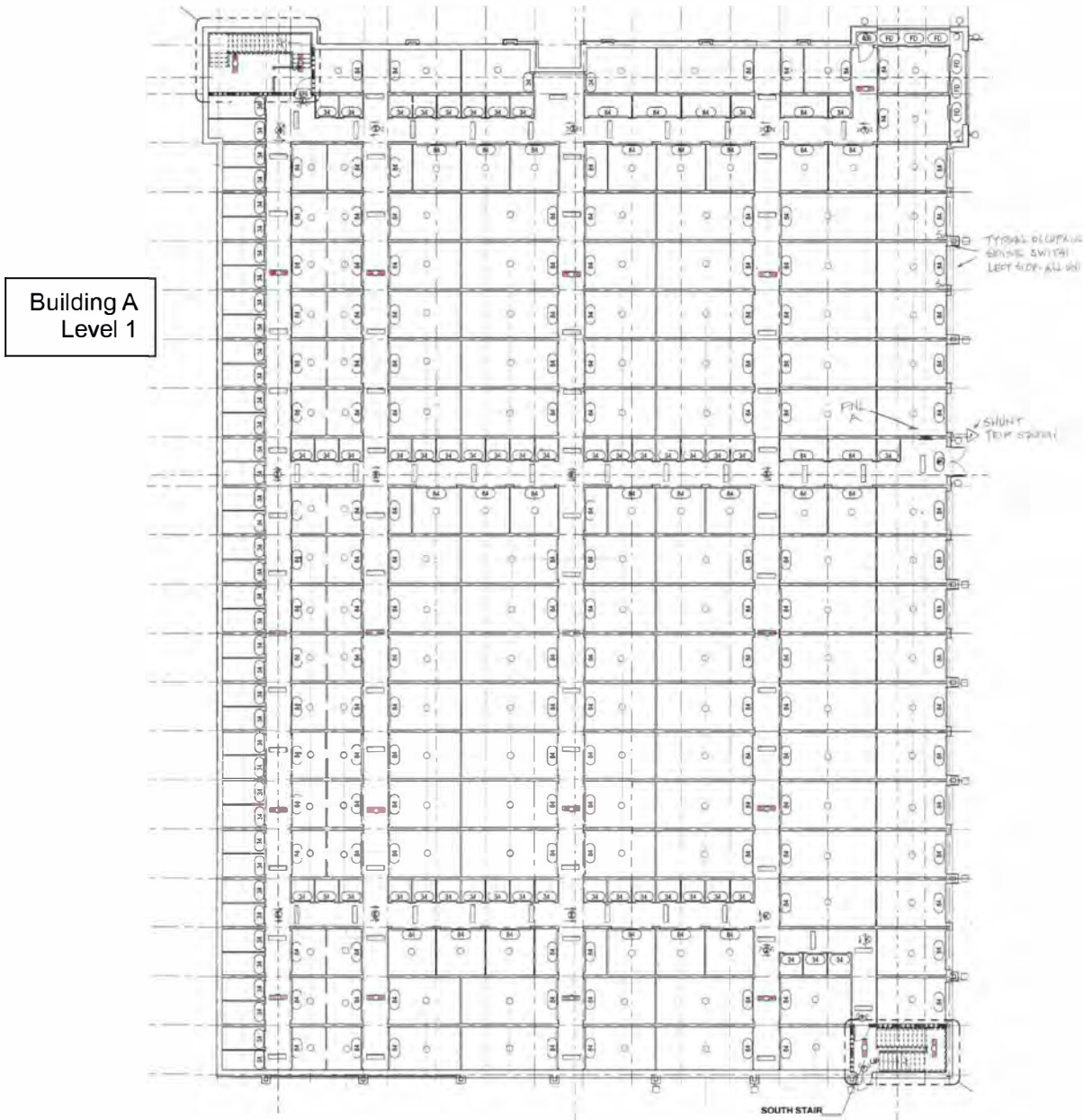


Figure 4 – Unit Mix

Hillside Development and Grading Analysis

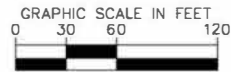
Per Washoe County Development Code Section 110.424.05(a)(1), properties with 15 percent or greater slope on 20 percent or more of the site is subject to hillside development. This applies to this project as approximately 44% of the site has more than 15% slope (see Table 1 below). However, the proposed development footprint has been designed to follow the existing terrain and avoid areas of the site with greater than 20% slope (see Figure 6). This calculates to an approximately 68% developable area of the site (Figure 7). The deepest cut is 18.48 ft and greatest fill is less than 10' with net import of 2,079 cubic yards (Figure 8).

MOUNT ROSE MINI STORAGE

SLOPE ANALYSIS MAP
Washoe County
March 2024

NOTE: THIS PLAN IS CONCEPTUAL IN NATURE

- APPROXIMATELY 44% OF SITE (12 195 ACRES) IS OVER 15% IN SLOPE
- APPROXIMATELY 16% OF SITE (10 798 ACRES) IS OVER 30% IN SLOPE



Slopes Table			
Number	Minimum Slope	Maximum Slope	Color
1	0.00%	5.00%	Green
2	5.00%	10.00%	Blue
3	10.00%	15.00%	Yellow
4	15.00%	30.00%	Red
5	30.00%	Vertical	Dark Red

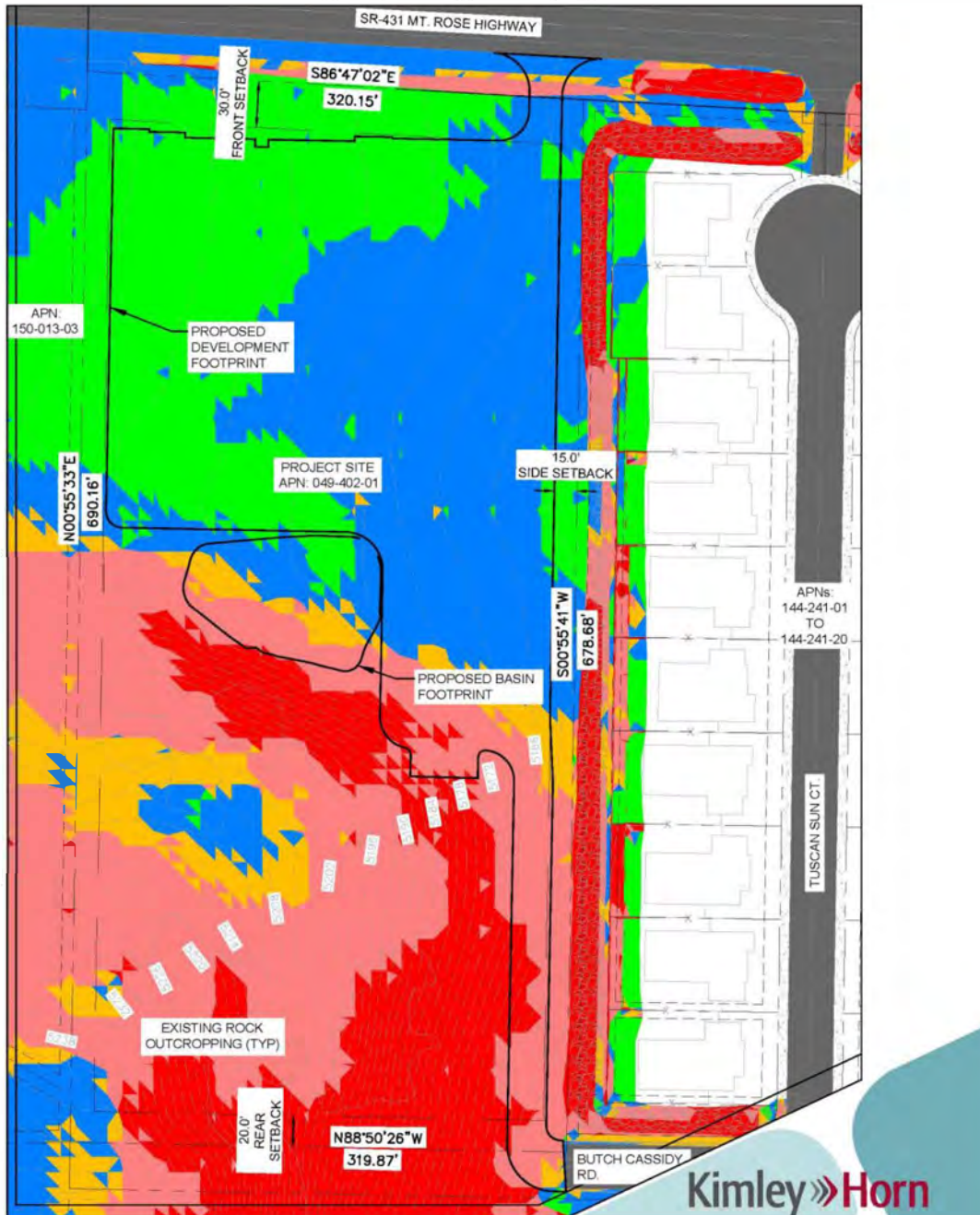
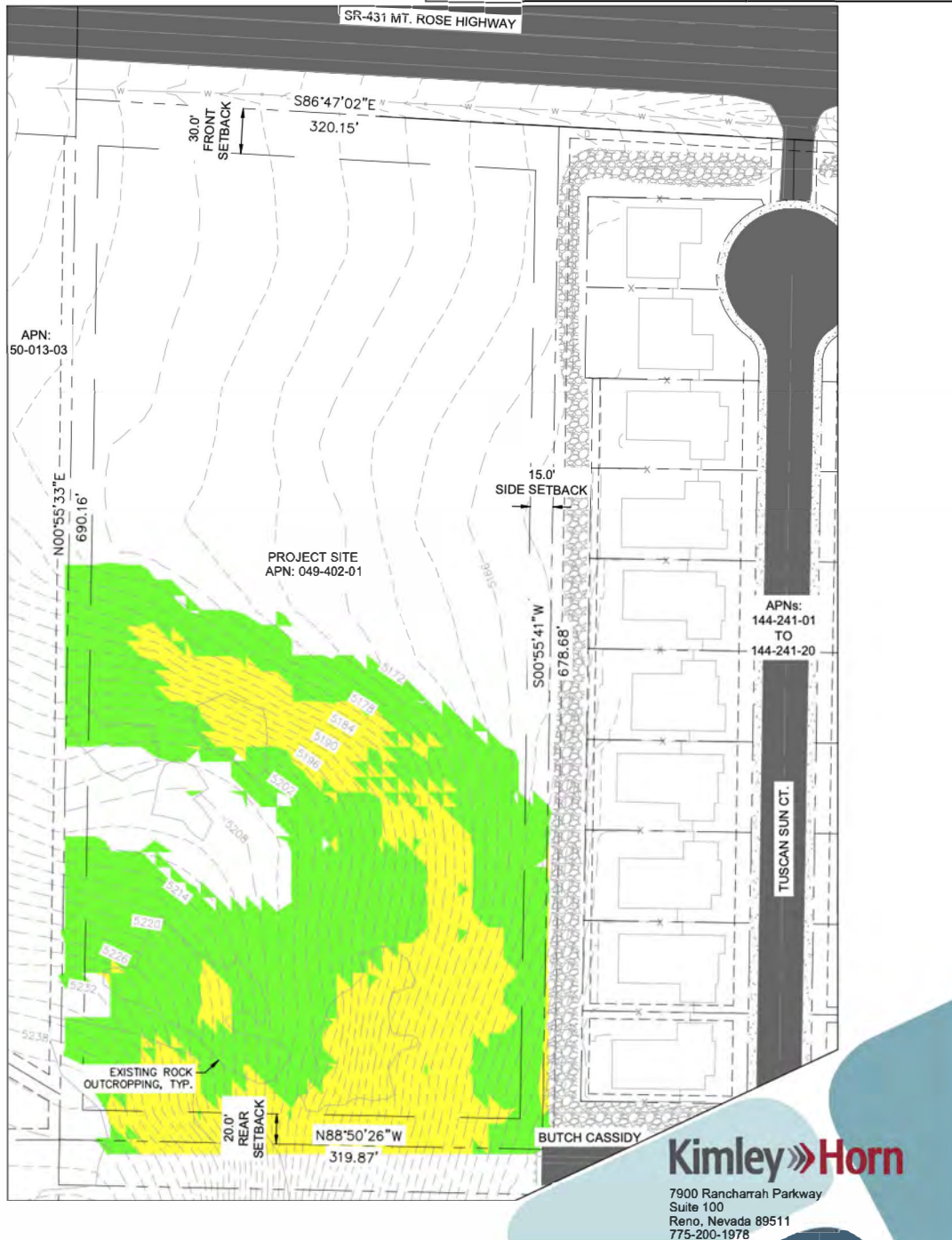


Figure 6 – Slope Map

Number	Minimum Slope	Maximum Slope	Area (SF)	Color
1	15.00%	30.00%	60,834.90	Green
2	30.00%	Vertical	34,778.15	Yellow

	AREA	
	SQUARE FOOT	ACRES
TOTAL PARCEL	218,931.06	5.026
SETBACKS	(35,033.60)	(0.804)
SLOPE >30%	(34,778.15)	(0.798)
TOTAL BUILDABLE AREA	149,119.31	3.423
PERCENTAGE OF PROJECT SITE BUILDABLE AREA	68%	



Kimley»Horn
 7900 Rancharrah Parkway
 Suite 100
 Reno, Nevada 89511
 775-200-1978

Figure 7 – Developable Area Map

Number	Minimum Elevation	Maximum Elevation	Area	Color
1	-18.48	-10.00	1396.03	Red
2	-10.00	0.00	49668.64	Light Red
3	0.00	10.00	60385.82	Light Blue

Earthwork Quantities:
 Cut = 9,234 CY
 Fill = 7,155 CY
 Net (Import) = 2,079 CY



Figure 8 – Cut & Fill Map

The proposed development has been designed to encourage compatibility with the existing hillside by minimizing the development footprint. The proposed design is in conformance with the requirements outlined in Article 424 Hillside Development Section 110.424.15(a)(1-9) and Section 110.424.30. The proposed development is designed to meet the requirements outlined in Section 110.424.00 as outlined by the following responses in *italics*:

Hillside Development Findings

Section 110.424.00 Purpose. The purpose of this article, Article 424, Hillside Development, is to regulate hillsides in a manner different from regulation of flat terrain. This article establishes provisions for developing, preserving and protecting hillsides and ridgelines within Washoe County. The intent of these regulations is to protect the public health, safety and welfare by:

- (a) Minimizing use of slopes subject to instability, erosion, landslide, flood hazards or drainage problems;

The proposed development does essentially avoid disturbance on steeper slopes on the project site (located south) and concentrate the development to the northern end of the site. The development area is roughly 40% of the site due to the topography and the applicant willing to leave it be. Please refer to slope analysis and grading plans for more detail. Retaining walls have been placed in lieu of large slope cuts on site where indicated. Grading of steep slope areas on the site has been minimized as shown.

- (b) Minimizing the careless alteration of and disruption to the natural topography and landscape;

The proposed design will incorporate new and mature landscaping in accordance with Washoe County standards and complement the existing terrain. Disruption to the natural topography will be limited by minimal development on steep slopes. The very base of the hill on the south end is minimally graded and aggressively treated with walls to maintain the natural topography.

- (c) Providing safe and adequate vehicular and pedestrian access to and within hillside areas, including emergency access;

Vehicular access will be provided from Mount Rose Highway (State Route 431.) Pedestrian access will be provided to the future Mount Rose shared multi-use path (currently in construction). The hillside area is avoided and not part of the vehicle and pedestrian access plan.

- (d) Establishing stormwater runoff and erosion control techniques to minimize adverse water quality impacts resulting from non-point runoff;

The proposed design will use erosion control best management practices (BMP's) and will minimize water quality impacts from non-point runoff. The stormwater runoff is managed by use of a detention area on site. Please refer to site and grading plans.

- (e) Encouraging innovative grading techniques and building design which respond to the hillside terrain and natural contours of the land;

There is no better grading technique, site design, and building design than avoidance of a significant topographic feature with notable vegetative cover on it and rock outcroppings that are stellar to the eye. The proposed design is in conformance with the site development and grading standards in Section 110.424.30 and 110.424.35. The buildings have been designed to follow the existing terrain and minimize adverse impacts to the

environment.

- (f) Minimizing impacts on existing trees and vegetation which reduce erosion, stabilize steep hillsides, enhance visual quality, protect water quality and preserve critical watershed recharge areas;

The proposed development will minimally impact on the steeper areas of the site. As stated in prior response, the proposed approach will result in the least amount of impacts on vegetation, visual impacts and protect water quality. Landscaping, limited grading, and erosion control measures are proposed in accordance with Washoe County standards. Refer to the site and grading plans for more detail.

- (g) Encouraging the transfer of density to avoid hazardous areas and to protect environmentally sensitive and open space areas; and

There is no transfer of density as the applicant is not looking to get more density for the 60% loss of site to the south. It simply is a reduction of usable area. The development is limited to the flatter slope areas at the northern end of the site and largely avoids grading of the nearby hillside to the south. There are no environmentally sensitive areas within the site. Refer to slope analysis and developable area map for more detail.

- (h) Minimizing impacts on prominent ridgelines, significant viewsheds, canyons and visually prominent rock outcroppings which reflect the visual value and scenic character of hillside areas.

The proposed design will not impact any ridgelines, significant viewsheds, canyons or prominent rock outcroppings. There are no ridgelines. However, there is a prominent viewshed and prominent rock outcroppings that are preserved in this design effort.

Grading Standards

Section 110.424.35 Grading and Drainage Standards. This section sets forth development standards for grading and drainage of hillside and ridgeline properties.

- (a) Grading. These grading standards are applicable to hillside and ridgeline development only if a special use permit for grading is required pursuant to Washoe County Ordinance 811. The following standards are intended to preserve natural topographic features, foster resource preservation and minimize degradation of the visual character of hillsides:

- (1) Grading shall relate to the natural topography with the natural topography maintained to the greatest extent possible;

This standard has been met with grading as proposed and is intended to get the site closer to its natural condition. We are moving from disturbance to revegetation and restoration.

- (2) Where alteration to the natural topography is necessary, graded slopes shall be contoured to provide a smooth and gradual transition of grading and natural slopes, while maintaining the basic character of the terrain;

All grading (whether in cut or fill areas) is counter graded to provided smooth and gradual transitions. There are no slopes resulting from the grading plan. In addition, restoration of graded areas with revegetation, and planting will help with mitigation.

- (3) Standard pad grading or terracing which results in grading outside the building footprint and access area shall be discouraged;

This has been accomplished as there is no terracing of the pads. There are raised pads for placement of the home footprints to ensure proper drainage is provided around the homes. However, grading outside of the building footprint is 100 percent imperative for a project like this.

- (4) Grading of knolls, ridgelines or toes of slopes shall be rounded to conform with the natural grade and to provide a smooth transition to the natural slope;

The notion of grading of knolls or ridgelines being proposed in the grading plan is not relevant to anything in this grading plan. The predominant land form of this site is using the flat land near the north end of the property. Most finished slopes are designed with a smooth transition to the natural slope with exception of the wall at the south end of the site that is a unique treatment to minimize disturbance area.

- (5) Grading shall create varying gradients in order to avoid a “manufactured” appearance;

Similar to above, our approach to vary gradients is to avoid a manufactured appearance in the proposed contours. This will be largely accomplished with the revegetation and planting. Also, while the isolated part of the grading using the retaining walls does not constitute manufactured slope. It has a purpose to minimize disturbance with a more vertical treatment.

- (6) Grading in environmentally sensitive habitat areas shall occur only when necessary to protect, maintain, enhance or restore the habitat; and

There are no environmentally sensitive areas on the site. This grading SUP is intended to simulate a more native condition of the site. This is the key point in the overall grading discussion given there are no significant water ways, drainageways, vegetation including trees and shrubs, wildlife or rock outcroppings on the property.

- (7) A slope stability and scarring mitigation plan, certificated by the project engineer, shall be reviewed and approved by the Director of Community Development and the Public Works Department prior to initiation of grading.

There is no issue of slope stability and scarring from this grading plan. The applicant agrees with this requirement.

- (b) Drainage and Erosion Control. All hillside development shall satisfy current Washoe County Code for drainage and erosion control.

The project will satisfy code requirements for drainage and erosion control. We accept that preventive measures on drainage and erosion control that satisfy the applicable articles of the development code can and will be met.

Section 110.438.45 Grading of Slopes. The standards in this section shall apply to all grading for subdivision improvements, special use permits, or other discretionary permits. The standards in this section shall also apply to all grading for building and grading permits upon or adjacent to lots less than or equal to five (5) acres in size, and to all grading within one hundred (100) feet of all property lines on parcels greater than five (5) acres in size.

- (a) Grading shall not result in slopes in excess of, or steeper than, three horizontal to one vertical (3:1) except as provided below:
 - (1) Storm drainage improvements.
 - (2) Cut and fill slopes less than thirty (30) inches in height.
 - (3) Cut slopes proposed to be located behind civic, commercial and industrial buildings, when the cut slope is shorter than and substantially screened by the proposed building. Such slopes are subject to approval of a Director's Modification of Standards by the Director of Community Development.
 - (4) The County Engineer may waive this requirement for up to fifteen (15) percent of the length of the cut and/or fill where the presence of rock or, in his determination, other practical hardships exists.

This project does not propose any slopes greater than 3:1, thus meeting this standard.

- (b) Within the required yard setbacks fills shall not differ from the natural or existing grade by more than forty-eight (48) inches (see Figure 110.438.45.1).

The standard is met with current project design, as the setback fills do not differ from the existing grade by more than 48 inches.

- (c) Finish grading shall not vary from the natural slope by more than ten (10) feet in elevation. Exposed finish grade slopes greater than ten (10) feet in height may be allowed upon the approval of a director's modification of standards by the Director of Community Development upon recommendation by the County Engineer.

This is not applicable as a practical matter as the cuts and fills are due to creating some usable pad areas and not cutting hills, or knolls, or significant landforms. The project will require a maximum cut of 18' and a maximum fill of 7' in height to achieve a uniform finished grade for the building pad locations.

Special Use Permit Findings

Section 110.810.30 Findings. Prior to approving an application for a special use permit, the Planning Commission, Board of Adjustment or a hearing examiner shall find that all of the following are true:

- (a) Consistency. The proposed use is consistent with the action programs, policies, standards and maps of the Master Plan and the applicable area plan;

The character statement of the Forest Area Plan emphasizes that commercial development must be located within the Mount Rose Scenic Highway Commercial Overlay District with careful considerations around visual impact and safety. The proposed storage facility requires a Special Use Permit for the NC zone but is consistent with the plan's goals and the best fit for the intended vision for the project site.

- (b) Improvements. Adequate utilities, roadway improvements, sanitation, water supply, drainage, and other necessary facilities have been provided, the proposed improvements are properly related to existing and proposed roadways, and an adequate public facilities determination has been made in accordance with Division Seven;

One public road will be provided to serve the site as shown in the project site plan. The site will be served by existing wet utility systems. Any additional utility improvements needed will be provided upon construction. Drainage improvements are outlined in the attached civil plan set.

- (c) Site Suitability. The site is physically suitable for the type of development and for the intensity of development;

The site has steep slopes to the south of the site that the development avoids, keeping the development to approximately 40% of the site area to minimize grading. This portion of the site has minimal area of greater than 20% slope and is suitable for the proposed development.

- (d) Issuance Not Detrimental. Issuance of the permit will not be significantly detrimental to the public health, safety or welfare; injurious to the property or improvements of adjacent properties; or detrimental to the character of the surrounding area; and

Issuance of the permit has no conceivable detriment to the public health, safety, or welfare; is not injurious to the property or improvements of adjacent properties; and is not of detrimental character to the surrounding area.

- (e) Effect on a Military Installation. Issuance of the permit will not have a detrimental effect on the location, purpose or mission of the military installation.

Issuance of the permit will not have a detrimental effect on the location, purpose, or mission of a military installation.

Forest Area Plan Analysis

Beyond its Washoe County land use designation, the project site is located in the area managed by the Forest Area Plan. Further, it is within the Mount Rose Scenic Highway Commercial Overlay District (MRHSCOD). The following sections discuss the themes and policies associated with the project as property within the Mount Rose Scenic Corridor and with a Commercial Land Use regulatory zone.

F.3.4 The intent of the MRSHCOD is to ensure that commercial development in and adjacent to the Mt. Rose Scenic Highway will:

- a. Contribute to the community character, promote neighborhood, and create a sense of place founded in the quality of life that comes with environmental and community responsibility.

The proposed development adds commercial use to an area specifically intended for this use. The development provides a needed service and employment to benefit the local community and is designed to integrate with the surrounding visually.

- b. Be compatible with and enhance the scenic quality of the Mt. Rose Highway corridor.

The site preserves the viewshed and rock outcropping that are part of the site by using hillside adaptive architecture and keeping development to the flattest portion of the site. Building materials are used that blend with the natural surrounding and are consistent with the surrounding.

- c. Enhance the safety of SR 431.

The proposed road accessed via SR 431 is designed in compliance with all safety standards for traffic improvements.

- d. Ensure that architectural styles enhance and contribute to the gateway function of the site and function to present a unified, integrated appearance.

The architectural design of the proposed development include materials and massing that present a unified appearance that blends with the natural features of the surrounding scenic corridor while creating a gateway visual appropriate for the area.

F.3.5 The Washoe County Development Code will further incorporate and describe this district. MRSHCOD Development Criteria:

- a. To promote a sense of neighborhood, to promote the functional and aesthetic integration of commercial uses with the community, and to promote the efficient use of resources and infrastructure in the Forest planning area, Commercial development proposals should include a residential component, or be closely integrated with nearby residential development. In order to facilitate this policy, special use permits to establish residential uses in commercial regulatory zones will not be required in the Forest planning area.

The proposed commercial development is directly adjacent to residential and places an appropriate buffer between the sites. The site is not suitable to include residential development given the slope constrains and reduced developable area.

- b. Commercial and mixed-use development proposals must show how the scenic quality of the Mt. Rose Highway will be preserved or improved, and must be consistent with all Goals and Policies and Development Code provisions relating to the Mt. Rose Scenic Corridor.

The commercial development is intentionally designed to preserve the viewshed of the surrounding hillside and is consistent with all Goals and Policies and Development Code provisions relating to the Mount Rose Schenic Corridor.

- c. Site development plans, including landscape plans, must be reviewed by the Washoe County Design Review Committee for consistency with the standards and guidelines established under this goal.

Site development and landscape plans are included with this application in Appendix B for review by the Washoe County Design Review Committee. The plans are consistent with the standards and guidelines of this goal.

- d. Ingress, egress, and internal circulation must be designed to improve overall traffic safety, improve access for affected adjacent property owners, consolidate and minimize access to SR 431, promote pedestrian and cycling activity, and mitigate any negative impact to existing development.

Internal circulation is designed to be as minimal as possible to allow for safe access and circulation while mitigating negative impacts associated with traffic. A pedestrian sidewalk is included in the site plan to accommodate the future Mount Rose shared multi-use path (currently in construction).

- e. At the request of the Department of Public Works, development proposals shall submit traffic reports and mitigation plans to the Regional Transportation Commission, Nevada Department of Transportation and the Washoe County Departments of Public Works and Community Development for review and approval prior to the approval of tentative maps or the issuance of building permits for the project. Any safety related traffic system improvements must be constructed commensurate with the development authorized as part of the tentative map or building permit.

A traffic report is included with this proposal in Appendix B for review prior to the issuance of any building permit for the project.

- f. A consistent architectural style shall be used to unify these parcels. This unifying style should recognize the important gateway function of the property in the region. Building materials shall be chosen for their ability to blend with the landscape and should emphasize the use of wood, stone, stucco or related materials.

Proposed building materials include brick and stone to blend with the landscape and provide a style appropriate for the gateway function of this region.

- g. The view of the property, particularly the view shed from SR 431 shall be designed such that site design, architectural styles, lighting, roadways, infrastructure, landscaping and signage blend with the natural features of the land and create a sense of place that is scenic, compatible with the local environment, and establishes a safe and welcoming neighborhood atmosphere.

The view of the property and the viewshed are visually consistent through the use of design in building materials, massing, landscaping, and lighting. The steeper hillside slopes and rock outcroppings within the site are preserved to further preserve the natural features of the land. See the Architectural Set included as an attachment in Appendix A of this application.

- h. Whenever possible given existing topography, the site design and architectural style should utilize hillside adaptive architecture.

The site design and architectural style utilize hillside adaptive architecture to limit the scenic and environmental impact of development on slopes, hilltops and ridges.

- i. A tree restoration and preservation plan, prepared by a certified landscape architect, shall be submitted with any site plan or tentative map proposals that propose the removal of 4 or more trees. A minimum of 2 inches diameter at breast height is required for any tree used for restoration, and a maximum of 75% of trees used for restoration may be at this minimum requirement.

There are no trees that are proposed to be removed on this site with this development.

- j. Prior to their incorporation into the Development Code, the policies established under Goal Three will be implemented through development agreements, tentative map conditions, improvements plans, CC&Rs, deed restrictions, or other methods deemed as appropriate by the Director of Community Development. When appropriate, Washoe County staff shall establish the implementation measures as conditions of tentative map and site plan approval.

The policies established under Goal Three that are applicable to this proposed development may be implemented through conditions of approval for the site plan.

Hillside Policies

Goal Seven: Maintain open vistas and minimize the visual impact of hillside development in a manner that implements the community character described above.

Policies

F.7.1 With the exception of temporary infrastructure for construction projects, Washoe County will require the underground placement of new utility distribution infrastructure within the Forest planning area. Utility transmission facilities will be subject to a special use permit. In considering whether to grant a special use permit, or in consideration of any conditions, including underground placement, which may be placed upon an approval, the Planning Commission will utilize the best available information, including but not limited to, the most recent Regional Utility Corridor Report and any Environmental Impact Statement or other study undertaken regarding the proposal. The County will seek funding sources and establish an implementation plan in conjunction with local utility companies to underground the power lines in the Scenic Corridor.

There is an existing fiber optic utility line at the north edge of the site, and no new above-ground utility distribution infrastructure is proposed.

F.7.2 The Washoe County Departments of Community Development and Public Works will establish and oversee compliance with design standards for grading that minimize the visual impact of all residential and non-residential hillside development, including road cuts and driveways. See Policy 2.1 regarding grading under Goal Two.

The grading plan for this project complies with all grading standards, see Grading Plan in Appendix B.

F.7.3 The grading design standards referred to in F.7.2 are intended to, at a minimum, ensure that disturbed areas shall be finished, fill slopes will not exceed a 3:1 slope, and that hillside grading will establish an undulating naturalistic appearance by creating varying curvilinear contours.

There are no fill slopes that will exceed a 3:1 slope in the proposed plan, and the grading will create varying contours to have a naturalistic appearance.

F.7.4 When necessary to mitigate the impact of road cuts, driveways and similar features on prominent hillsides, staff may require the installation of landscaping that will significantly soften the visual impact within three years of installation. Maintenance plans for these landscaped areas may be required.

The proposed development as designed includes landscaped buffer areas to soften visual impact along Mount Rose Highway and the adjacent residential neighborhood. Both landscaped areas will be maintained as necessary.

F.7.5 County will review its revegetation Policy, require additional funds be set aside for revegetation, and mandate 80% reestablishment of vegetation prior to release of the bonds.

Areas requiring revegetation will be 80% reestablished before the release of bonds noted above.

Appendix A

Application Materials

Washoe County Development Application

Property Owner Affidavit

Special Use Permit Checklist

Special Use Permit – Supplemental Information

Special Use Permit – Supplemental Information for Grading

Washoe County Fee Schedule

Washoe County Treasurer – Tax Payment Records

Washoe County Development Application

Your entire application is a public record. If you have a concern about releasing personal information, please contact Planning and Building staff at 775.328.6100.

Project Information		Staff Assigned Case No.: _____	
Project Name: Supreme Storage			
Project Description: A climate controlled storage facility with two buildings and a gross building area of 64,250 SF.			
Project Address: 3900 Mount Rose Hwy, Reno, NV 89511			
Project Area (acres or square feet): 75,000 SF			
Project Location (with point of reference to major cross streets AND area locator): Approximately 500 ft east of the intersection of Mount Rose Highway and Thomas Creek Road			
Assessor's Parcel No.(s):	Parcel Acreage:	Assessor's Parcel No.(s):	Parcel Acreage:
049-402-01	5.026		
Indicate any previous Washoe County approvals associated with this application: Case No.(s).			
Applicant Information (attach additional sheets if necessary)			
Property Owner:		Professional Consultant:	
Name: Winterberg 2015 Family Trust		Name: John Krmpotic, KLS Planning & Design	
Address: 5395 Goldenrod Dr		Address: 201 W Liberty St	
Reno, NV	Zip: 89511	Reno, NV	Zip: 89501
Phone:	Fax:	Phone: 775-852-7606	Fax:
Email:		Email: johnk@klsdesigngroup.com	
Cell:	Other:	Cell: 775-857-7710	Other:
Contact Person:		Contact Person: John Krmpotic	
Applicant/Developer:		Other Persons to be Contacted:	
Name: Joe Strobele, Managing Member		Name:	
Address: CPD Mt Rose LLC		Address:	
	Zip:		Zip:
Phone:	Fax:	Phone:	Fax:
Email: jstrobele@cp-investments.com		Email:	
Cell: 206-910-8282	Other:	Cell:	Other:
Contact Person:		Contact Person:	
For Office Use Only			
Date Received:	Initial:	Planning Area:	
County Commission District:		Master Plan Designation(s):	
CAB(s):		Regulatory Zoning(s):	

Special Use Permit Application Supplemental Information

(All required information may be separately attached)

1. What is the project being requested?

A Special Use Permit for a storage facility that qualifies as Hillside Development and exceeds Major Grading Permit Thresholds per Washoe County Development Code.

2. Provide a site plan with all existing and proposed structures (e.g. new structures, roadway improvements, utilities, sanitation, water supply, drainage, parking, signs, etc.)

See site plan attached in Appendix A of this application.

3. What is the intended phasing schedule for the construction and completion of the project?

This is a single phase project. Construction timeline has not been established yet.

4. What physical characteristics of your location and/or premises are especially suited to deal with the impacts and the intensity of your proposed use?

The proposed project uses the developable area of the site and avoids areas of steep slopes to minimize grading. The intensity is similar to those of the site surrounding.

5. What are the anticipated beneficial aspects or affects your project will have on adjacent properties and the community?

Improved visual impacts, addition of jobs and services for the local community, and pedestrian access to the future Mt Rose shared multi-use path.

6. What are the anticipated negative impacts or affect your project will have on adjacent properties? How will you mitigate these impacts?

The negative impacts are anticipated to be minimal. A landscape buffer is provided along the adjacent residential.

7. Provide specific information on landscaping, parking, type of signs and lighting, and all other code requirements pertinent to the type of use being purposed. Show and indicate these requirements on submitted drawings with the application.

See attached site and landscape plans included in this application for details and requirements.

8. Are there any restrictive covenants, recorded conditions, or deed restrictions (CC&Rs) that apply to the area subject to the special use permit request? (If so, please attach a copy.)

<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
------------------------------	--

9. Utilities:

a. Sewer Service	TMWRF
b. Electrical Service	NV Energy
c. Telephone Service	ATT or Charter
d. LPG or Natural Gas Service	NV Energy
e. Solid Waste Disposal Service	Waste Management
f. Cable Television Service	Charter Cable
g. Water Service	TMWA

For most uses, Washoe County Code, Chapter 110, Article 422, Water and Sewer Resource Requirements, requires the dedication of water rights to Washoe County. Please indicate the type and quantity of water rights you have available should dedication be required.

h. Permit #	TBD	acre-feet per year	
i. Certificate #	TBD	acre-feet per year	
j. Surface Claim #	TBD	acre-feet per year	
k. Other #	TBD	acre-feet per year	

Title of those rights (as filed with the State Engineer in the Division of Water Resources of the Department of Conservation and Natural Resources).

N/A

10. Community Services (provided and nearest facility):

a. Fire Station	Truckee Meadows Fire Station #36
b. Health Care Facility	Saint Mary's Galena Urgent Medical Center
c. Elementary School	Hunsberger Elementary School
d. Middle School	Herz Middle School
e. High School	Galena High School
f. Parks	Whites Creek Park
g. Library	South Valleys Library
h. Citifare Bus Stop	Herz Boulevard / Mount Rose Highway

**Special Use Permit Application
for Grading
Supplemental Information**
(All required information may be separately attached)

1. What is the purpose of the grading?

The purpose of the grading is to create uniform building pads for the two buildings and roadways to access the site.

2. How many cubic yards of material are you proposing to excavate on site?

A total of 9,234 cubic yards is planned to be excavated.

3. How many square feet of surface of the property are you disturbing?

3.42 acres

4. How many cubic yards of material are you exporting or importing? If none, how are you managing to balance the work on-site?

Net import will be 2,079 cubic yards of material.

5. Is it possible to develop your property without surpassing the grading thresholds requiring a Special Use Permit? (Explain fully your answer.)

Due to the irregular sloping existing on the site, it is not possible to develop without surpassing these grading thresholds. However the project was designed to minimize grading of areas sloped greater than 20%.

6. Has any portion of the grading shown on the plan been done previously? (If yes, explain the circumstances, the year the work was done, and who completed the work.)

None of the grading on the plan has been done previously.

7. Have you shown all areas on your site plan that are proposed to be disturbed by grading? (If no, explain your answer.)

Yes, all areas on the site plan that are proposed to be disturbed by grading are shown on the plan.

8. Can the disturbed area be seen from off-site? If yes, from which directions and which properties or roadways?

No, all of the disturbed area visible from off-site will be hidden with the proposed development. While on site, there will be one visible retaining wall at the south of the site.

9. Could neighboring properties also be served by the proposed access/grading requested (i.e. if you are creating a driveway, would it be used for access to additional neighboring properties)?

Neighboring residential parcels will be served by the commercial business that is made possible by this grading. The roadway and development is meant to be kept separate from the subdivision. A pedestrian walkway is included in the development.

10. What is the slope (horizontal/vertical) of the cut and fill areas proposed to be? What methods will be used to prevent erosion until the revegetation is established?

The area of grading with most slope at the south of the site will use a vertical retaining wall, others will be 3:1 slope.

11. Are you planning any berms?

Yes	No X	If yes, how tall is the berm at its highest?
-----	------	--

12. If your property slopes and you are leveling a pad for a building, are retaining walls going to be required? If so, how high will the walls be and what is their construction (i.e. rockery, concrete, timber, manufactured block)?

Retaining walls are proposed at 10' height and made of rockery or other engineered +

13. What are you proposing for visual mitigation of the work?

The grading will use varying contours and landscaping or revegetation of native species to visually mitigate the work.

14. Will the grading proposed require removal of any trees? If so, what species, how many and of what size?

The grading will not require the removal of any trees.

15. What type of revegetation seed mix are you planning to use and how many pounds per acre do you intend to broadcast? Will you use mulch and, if so, what type?

Revegetation seed mix is to be determined, see landscape plan.

16. How are you providing temporary irrigation to the disturbed area?

All landscaping will be automatically irrigated. A reduced-pressure-type backflow preventer will be provided on the irrigation system as required per code.

17. Have you reviewed the revegetation plan with the Washoe Storey Conservation District? If yes, have you incorporated their suggestions?

We will review the Washoe Storey Conservation District revegetation plan in this process.

18. Are there any restrictive covenants, recorded conditions, or deed restrictions (CC&Rs) that may prohibit the requested grading?

Yes	No <input checked="" type="checkbox"/>	If yes, please attach a copy.
-----	--	-------------------------------

Appendix B

Civil Reports/Studies

Geotechnical Desktop Study

Trip Generation Letter

GEOTECHNICAL DESKTOP STUDY
3900 MOUNT ROSE HIGHWAY
APN 049-402-01
RENO, WASHOE COUNTY, NEVADA



**CONSTRUCTION
MATERIALS
ENGINEERS, INC.**



PREPARED FOR:

Kimley»»Horn

AUGUST 2023
FILE: 3271

August 1, 2023
File: 3271

Chris Waechter, PE
Kimley Horn
david.giacomin@kimley-horn.com

**RE: Geotechnical Desktop Study
Proposed Mini Storage
3900 Mount Rose Highway (APN 049-402-01)
Reno, Washoe County, Nevada**

Dear Mr. Waechter:

Kimley Horn is part of a team evaluating development of a commercial Mini Storage on Washoe County APN 049-402-01, located at 3900 Mount Rose Highway, in Reno, Nevada. This project is currently in the due diligence phase for land purchase. Enclosed is CME's Geotechnical Desktop Study, which provides a summary of our geotechnical findings related to the current proposed development at the subject. The results of the geotechnical desktop study are intended to provide the client with a higher level of geotechnical understanding for due diligence and planning purposes; this desktop study does not constitute a design level geotechnical report.

After client acquisition, the parcel shall undergo a design level geotechnical investigation which may include subsurface exploration, laboratory testing, engineering analyses, and reporting relevant to the proposed site improvements. CME can provide a proposal to provide the design level geotechnical investigation at a discounted cost for our in-house services due to our current involvement with the project. CME is a long established and reputable geotechnical engineering firm in the area with a proven track record of working with clients for overall project success.

Thank you for the opportunity to provide our services and look forward to working on future endeavors together. Please contact the undersigned should you have any questions or require additional information.

Sincerely,

CONSTRUCTION MATERIALS ENGINEERS, INC.

Nicholas R. Anderson, PE, GE
Geotechnical Project Manager
nanderson@cmenv.com
Direct: 775-737-7578
Mobile: 916-705-1959

Launie A. McRoberts, EI
Engineering Intern
lmcroberts@cmenv.com
Direct: 775-378-3133

NRA:LAM:lam
v:\active\3271\report\final\geotechnical desktop study - 3900 mount rose highway.docx

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APPENDICES

Appendix A – Quaternary Fault Evaluation

1.0 EXECUTIVE SUMMARY

Based on the reviewed reports and mapped geologic conditions outlined in Section 2.0, Table 1 summarizes our geotechnical, geologic, and seismic hazard considerations .

Table 1: Geotechnical/Geologic/Seismic Summary of Findings		
Topic	Commentary	Refer to Section
Topography	<p>The proposed development area will occur on the northern half of the subject site. This area slopes moderately from west to east at an approximate grade of 5 percent.</p> <p>The southern half of the site is anticipated to remain undeveloped as the steeply sloping hillside will result in significant and costly mass grading if used to support a mini-storage facility.</p>	Section 2.0
Flood Hazard	FEMA maps the site as an area of minimal flood hazard.	Section 3.2
Faulting Concern	The nearest mapped fault trace is located approximately 800 feet to the northwest of the site. This fault is not anticipated to impact on the proposed development.	Section 3.4
Shallow Groundwater	Groundwater is anticipated to lie at a depth greater than 20 feet. However, seasonal seepage during spring runoff may be observed during construction.	Section 3.3 and 3.1
Bedrock	Dacite bedrock mapped on the southern portion of the site is anticipated to be slightly to moderately weathered. Dacite bedrock may be located on the northern portion of the site, however this bedrock horizon (if present) is anticipated to be encountered at depths greater than 15 feet.	Section 3.1 and 3.3
Oversized Materials (Cobbles and Boulders)	<p>Subsurface soil is anticipated to be predominantly silty sand with gravel, cobbles, and boulders. Cobbles and boulders will impact confined excavations and mass grading. Oversized materials (i.e., greater than 4-inches nominal diameter) may comprise 10 to 30 percent of the total volume of the soil mass. Boulder diameters 5 to 6-feet in diameter may be present within the underlying soil matrix.</p> <p>When estimating earthwork quantities, a higher percentage for shrinkage will need to be accounted for where onsite materials are proposed for reuse (i.e., cut/fill pads).</p>	Section 3.1 and 3.3
Liquefaction Potential	Due to the dense nature of the geologic unit, and depth to groundwater, liquefaction hazards for at the subject site are not a geotechnical concern.	Section 3.1 and 3.3
Expansive Soil	The potential for near surface clay (i.e., expansive soil) is considered very low.	Section 3.1 and 3.3
Undocumented or Uncontrolled Fill	Uncontrolled or undocumented fill is not anticipated at the site.	Section 2.0
Anticipated Foundation Considerations	<p>The underlying mapped geologic units exhibit a high shear strength in turn yielding a high bearing capacity.</p> <p>The native onsite soils will serve as an adequate bearing stratum for proposed foundations, slabs, or other site improvements.</p>	Section 2.0, 3.1, and 3.3
Pavement and Flatwork	Onsite soils are anticipated to exhibit R-values greater than 30 indicating the onsite soils will provide a still resistance to rutting and subgrade failures where pavement or flat work is proposed.	Section 3.1 and 3.3

2.0 EXISTING SITE CONDITIONS

2.1 SITE DESCRIPTION

The subject site is located within APN 049-402-01 at 3900 Mount Rose Highway in Reno, Washoe County, Nevada. Figure 1 presents the site location.



Figure 1: Site Location (Washoe County GIS)

The subject site consists of a rectangular, undeveloped, 5-acre lot located at the western terminus of Butch Cassidy Drive. Vegetation at the site consists of moderately dense to dense desert grasses and mature sagebrush up to 4 feet tall. Based on a review of [Washoe County GIS mapping](#), total topographic relief across the site is roughly 90 to 100 feet with the following generalized slope gradients:

- The northern half of the site slopes down from west to east at an approximate grade of 5 percent. The proposed development will be within the northern extents, along the more gently sloping terrain.
- The southern half of the site hosts a hill that slopes down to the northeast and east at a grade of approximately 30 percent.

2.2 FIELD RECONNAISSANCE

CME completed a field reconnaissance on June 26, 2023, to review and document pertinent site features which may impact the proposed development. At the time of the site visit, shoulder improvements were being performed by NDOT within the right-of-way along Mt. Rose Highway which is the northern boundary of the subject site and USANorth811 utility markings were visible on the roadway.

An underground fiber optic cable runs along the northern boundary of the property (refer to Photo 1), with underground water running parallel to the northern boundary within the paved shoulder of the Mt. Rose Highway (refer to Photo 2). Surface boulders with an estimated maximum nominal diameter of up to 5 feet, were located along the northern boundary of the site (refer to Photo 3) at the time of our visit. The remaining portions of the site remain undeveloped with dense vegetation.



Photo 1: Looking South at Fiber Optic Cable Marker



Photo 2: Looking East at Water Line



Photo 3: Looking South at Boulders



Photo 4: Looking Southeast at General Site

3.0 LITERATURE REVIEW AND PUBLISHED MAPPING

3.1 NEARBY GEOTECHNICAL REPORTS

Figure 2 presents the subject site in relation to existing geotechnical information contained in CME's internal geotechnical library, as indicated by the CME logo markers.



Figure 2: Nearby Geotechnical Explorations

Table 2 presents a summary of the reports reviewed and general proximity to the project site.

Table 2: Summary of Nearby Geotechnical Investigation Reports Reviewed				
Title	Date	Approximate Distance / Direction from Site	Approximate Depth of Exploration (ft)	Data Quality
Galena Market	10/01/1992	500 Feet / Northwest	14	Low to Medium
CareMeridian Reno Skilled Living-Phase II	7/13/2021	1,000 Feet / Northwest	15	Medium to Good
855 Redfield-Galena	01/01/1994	1,500 Feet / Southeast	25	Medium

3.2 FEMA FLOOD HAZARD MAPPING

A FEMA webtool¹ was used to assess flood hazard risk. The FEMA webtool was accessed on June 12, 2023, and FIRM Panel 32031C3245G (March 16, 2009) was reviewed. The subject site is mapped as Zone X (Unshaded) and has a minimal flood hazard. Figure 3 presents an excerpt of the map with a summary of the FEMA flood hazard category definitions.



FEMA Codes	Definition
Zone A or Zone V	High Flood Hazard (Lower elevation than the 100-year flood)
Zone B or Zone X (Shaded)	Moderate Flood Hazard (Elevation between the 100-year and 500- year flood)
Zone C or Zone X (Unshaded)	Minimal Flood Hazard (Higher elevation than the 500-year flood)

Figure 3: FEMA Map Excerpt

¹ <https://hazards-fema.maps.arcgis.com/apps/webappviewer/index.html?id=8b0adb51996444d4879338b5529aa9cd>

3.3 GEOLOGIC MAPPING

To assess the underlying mapped geologic conditions at the site, a USGS webtool² was used. An excerpt of the *Preliminary Revised Geologic Maps of the Reno Urban Area, Nevada* (Ramelli et al, 2011) is presented as Figure 4 below:

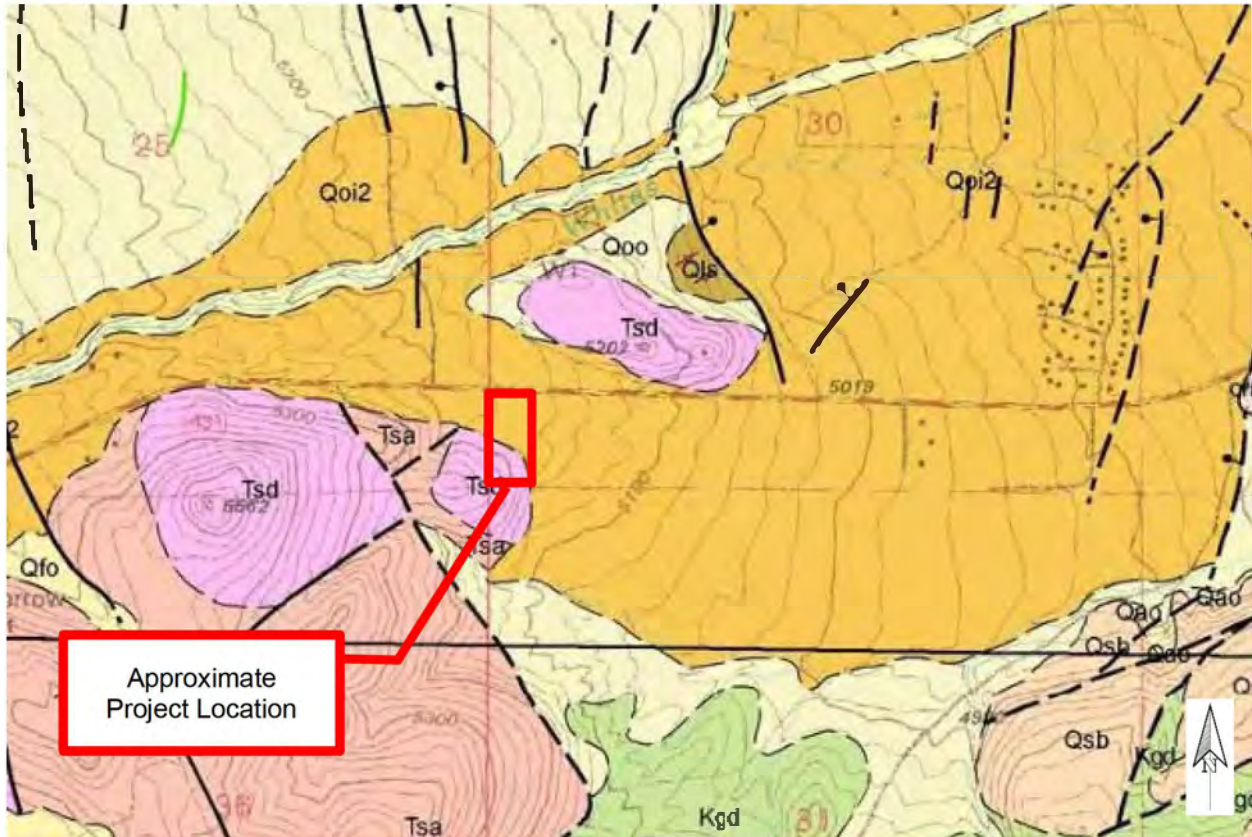


Figure 4: Geologic Map Excerpt

Based on the geologic mapping, the applicable geologic units are:

- **Qoi2** Intermediate Age outwash deposits (northern portion of the site). Outwash deposits in the vicinity of the project generally consists of sand and gravel with cobbles and boulders.
- **Tsd** Dacite of the Steamboat Hills (localized to the south/southwest portion of the site). The Steamboat Hills is a topographically prominent northeast-trending bedrock ridge that represents the southern extent of the fault-bounded Truckee Meadows basin.

² <https://ngmdb.usgs.gov/mapview/?center=-97,39.6&zoom=4>

3.4 FAULT MAPPING

To determine the location of mapped earthquake faulting trending through or near the project site, a review of the following published information was completed:

1. USGS Website: *Earthquake Hazards Program Quaternary Faults in Google Earth*;
2. The USGS Interactive Fault Map³; and
3. University of Nevada Reno Interactive Fault Map⁴.

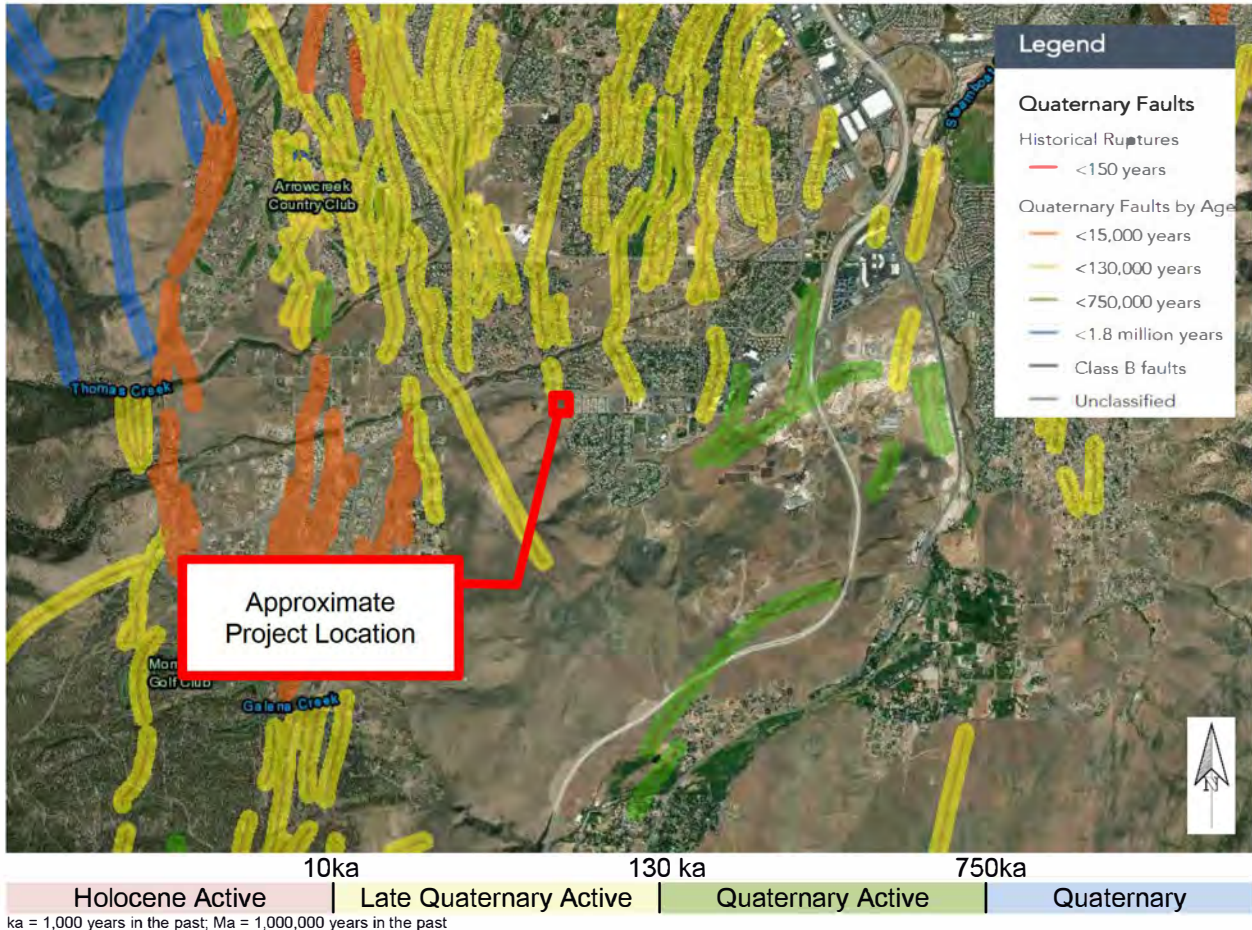


Figure 5: Fault Map Excerpt

The subject site lies within an area of dense faulting within the limits of the Mount Rose Fault zone. The potential for surface rupture hazards were assessed by Piedmont Geoseismic Services (PGS) as part of this current scope of work. PGS conducted a Phase 1 (Quaternary Fault Evaluation) study which included a literature review using a combination of geologic maps, aerial photographs, high-resolution LiDAR topographic data.

The closest mapped fault trace was observed on aerial photographs and LiDAR imagery and is determined to be on the order of 800 feet to the northwest of the northwestern corner of the subject site. Based on the results of the PGS analysis of the surface modification in the vicinity of the fault, the mapped geologic age of late Quaternary (movement within the last 130,000 years) was confirmed.

Based on the fault location and age, the fault does not pose a surface rupture hazard at the project site. Refer to Appendix A for the complete Quaternary fault evaluation report.

³ <https://usgs.maps.arcgis.com/apps/webappviewer/index.html?id=5a6038b3a1684561a9b0aadf88412fcf>

⁴ <https://gisweb.unr.edu/QuaternaryFaults>

4.0 LIMITATIONS

General Intent	<ul style="list-style-type: none">• The intent of this report is to provide preliminary geotechnical, geologic, and seismic information related to the feasibility of the project site. This report is not intended to be used for design or permitting purposes.• If the site will be developed or improved, a design level geotechnical investigation should be performed.
Warranties	<ul style="list-style-type: none">• CME makes no other warranties, either expressed or implied, as to the professional advice provided under the terms of this agreement and included in this report. Any use, reliance on, or decisions which a third party makes, based upon the information contained in this report, are the sole responsibility of such third parties. CME accepts no responsibility for damages, if any, suffered by any third party as a result of decisions made or actions based on this report.
Environmental Hazards Evaluation	<ul style="list-style-type: none">• Any evaluation of the site for the presence of surface or subsurface hazardous substances is beyond the scope of this study. When suspected hazardous substances are encountered during routine geotechnical investigations, they are noted in the exploration logs and reported to the client.

APPENDIX A



June 23, 2023

Construction Materials Engineers, Inc.
300 Sierra Manor Drive, Suite 1
Reno, NV 89511

ATTN: Nick Anderson, P.E.

Thomas L. Sawyer
Piedmont Geoseismic Services
HC72 Box 20233
Dyer, Nevada 89010

**SUBJECT: QUATERNARY FAULT EVALUATION
APN 049-402-01
Reno, Washoe County, Nevada**

Mr. Anderson,

Piedmont Geoseismic Services is pleased to submit the attached technical memorandum that summarizes a Quaternary fault evaluation of the above-referenced project site, located northwest of the west end of Butch Cassidy Drive in Reno, Washoe County, Nevada (APN 049-402-01). The purpose of the evaluation is to verify the existence, location and activity of a late Quaternary fault trace previously mapped in the vicinity of the project site (Figure 1). Specifically, to evaluate if the subject fault trace poses any surface rupture hazards at the project site.

The Quaternary fault evaluation was conducted by Mr. Thomas L. Sawyer, Seismic Hazards Specialist of Piedmont Geoseismic Services (PGS), Dyer, Nevada. In addition to reviewing geologic literature, the scope of this Quaternary fault Evaluation included examining aerial photographs, assessing LiDAR high-resolution topographic data, and conducting geologic field reconnaissance of the proposed project site and vicinity.

Sincerely,
PIEDMONT GEOSEISMIC SERVICES, INC.

Thomas L. Sawyer
Seismic Hazard Specialist

HC72 Box 20233 • Dyer, Nevada 89010
sawyer.pgs@gmail.com • (775) 772-5902



TECHNICAL MEMORANDUM

QUATERNARY FAULT EVALUATION
APN 049-402-01
Reno, Washoe County, Nevada

INTRODUCTION

This technical memorandum summarizes a Quaternary fault evaluation of the above-referenced project site, located northwest of the west end of Butch Cassidy Drive in Reno, Washoe County, Nevada (APN 049-402-01). The purpose of the evaluation is to verify the existence, location and activity of a late Quaternary fault trace previously mapped in the vicinity of the project site (Figure 1). Specifically, to evaluate if the subject fault trace poses any surface rupture hazards at the project site. The Quaternary fault evaluation was conducted by Mr. Thomas L. Sawyer, Seismic Hazards Specialist of Piedmont Geoseismic Services (PGS), Dyer, Nevada. In addition to reviewing geologic literature, the scope of this Quaternary fault Evaluation included examining aerial photographs, assessing LiDAR high-resolution topographic data, and conducting geologic field reconnaissance of the proposed project site and vicinity.

QUATERNARY FAULT EVALUATION

Research geologist with the Nevada Bureau of Mines and Geology (NBMG), Bonham and Rogers (1983), mapped a late Quaternary (movement in past 130,000 years) fault trace in the vicinity of the project site, north of the Mount Rose Highway 431 (Figure 1). This information was subsequently compiled by the U.S. Geological Survey (USGS, 2023), as well as, by the NBMG (2023). However, the location of the subject fault trace as originally mapped by Bonham and Rogers (1983) differs from that depicted in the regional compilation maps by the USGS and NBMG (see Figure 1).

Based on the geologic mapping of Bonham and Rogers (1983), the subject fault is short, north striking, and lies between the highway and Whites Creek. Along its length, the fault offsets Tahoe-aged (late Pleistocene) glacial outwash deposits (unit Q_{tm} of Bonham and Rogers, 1983). The subject fault trace appears to be the southern continuation of a longer fault trace, north of the creek, that exhibits down-west offsets of unit Q_{tm}, as well as, of older (late mid Pleistocene) Donner Lake glacial outwash deposits (unit Q_{dm} of Bonham and Rogers, 1983).

Large-scale (1:12,000), black and white, aerial photographs, obtained from the publication office of the NBMG in Reno, Nevada, were stereoscopically examined for



geomorphic features (e.g., fault scarps) indicative of Quaternary surface faulting. These photos were acquired under low-sun-angle conditions specifically to aid in the identification of such geomorphic features. The subject fault trace and that along strike to the north were found to be marked in the aerial photographs by west-facing, somewhat rounded fault scarps, particularly the trace to the north, which also is characterized by higher, more prominent fault scarps.

High-resolution LiDAR topographic data was compiled and used to construct a detailed shaded relief map of the project site and vicinity (see basemap in Figure 2). Like the aerial photographs, the shaded relief map was illuminated with a variety of low-oblique illumination angles to accentuate tectonic geomorphic features. However, due to the extent and magnitude of surface modifications north of Highway 431, the LiDAR data was only marginally useful in this area. The shaded relief map does reveal the overall character of fault scarps along the subject fault trace, as well as, the trace to the north.

To evaluate the possible southern extent of the subject fault trace, which if it exists would be closer to the project site than the mapped subject fault trace, LiDAR-based topographic profiles were constructed across the southern, along-strike projection of the subject fault trace south of Highway 431. The profile alignments, which also traverse east-west across the project site, are shown in Figure 2. Two representative topographic profiles are shown as Figure 3. The profiles were drawn on, or largely on, the depositional surface of unit Q_{tm}. The surface is shown as having a broad, slightly to very slightly convex longitudinal profile, consistent with the depositional setting of the project-site area in the late Pleistocene.

Similarly, surface modifications hampered the ability to identify detailed geomorphic features along the subject fault trace during the geologic field reconnaissance. The Q_{tm}-geomorphic surface south of Highway 431, however, was found to be largely unmodified at the project site and to the west. Here the Q_{tm} surface appears to be graded and unbroken, consistent with the examination of aerial photographs and analysis of LiDAR data.

CONCLUSIONS

Therefore, the existence and location, slip sense, and general late Quaternary activity of the subject fault trace, as originally depicted by Bonham and Roger (1983), were confirmed in this Quaternary fault evaluation, based on the examination of aerial photography and LiDAR data, as well as, geologic field reconnaissance. Furthermore, it is obvious that the subject fault trace is incorrectly located in the regional Quaternary fault compilation maps of the USGS and NBMG.



Thus, at its closest approach, the subject fault trace lies about 800 feet northwest of the northwest corner of the project site. At that distance, the subject fault does not pose a surface rupture hazard at the project site (APN 049-402-01).

Therefore, from the standpoint of surface-rupture hazards, we conclude that the project site is suitable for development.

RECOMMENDATIONS

-None.

CLOSURE

It has been a pleasure conducting this Quaternary fault evaluation for CME, Reno, Nevada in support of the proposed project at APN 049-402-01 in Reno, Washoe County, Nevada. If you have any questions, concerns or require further assistance, please contact us at your convenience.

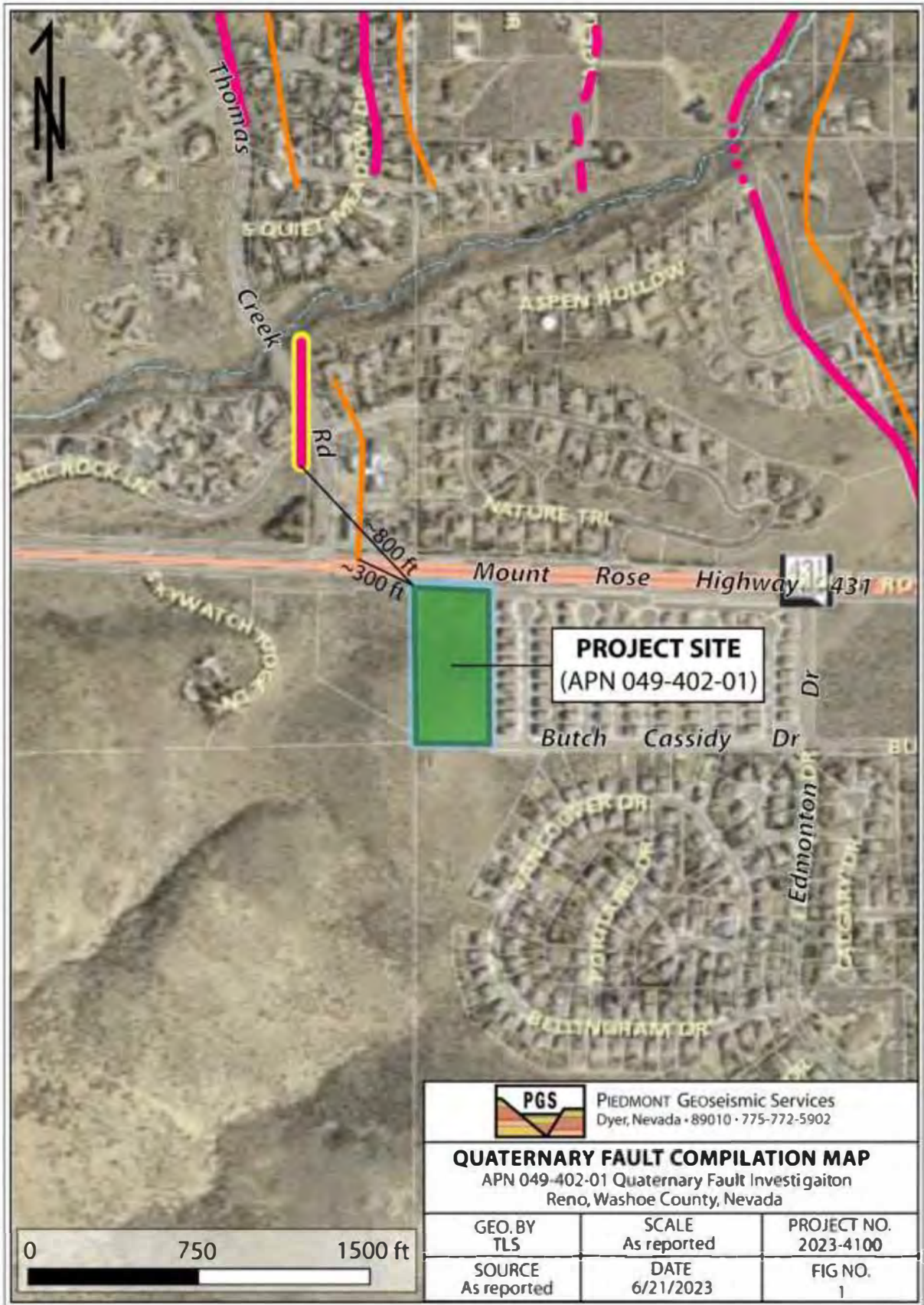
Sincerely,

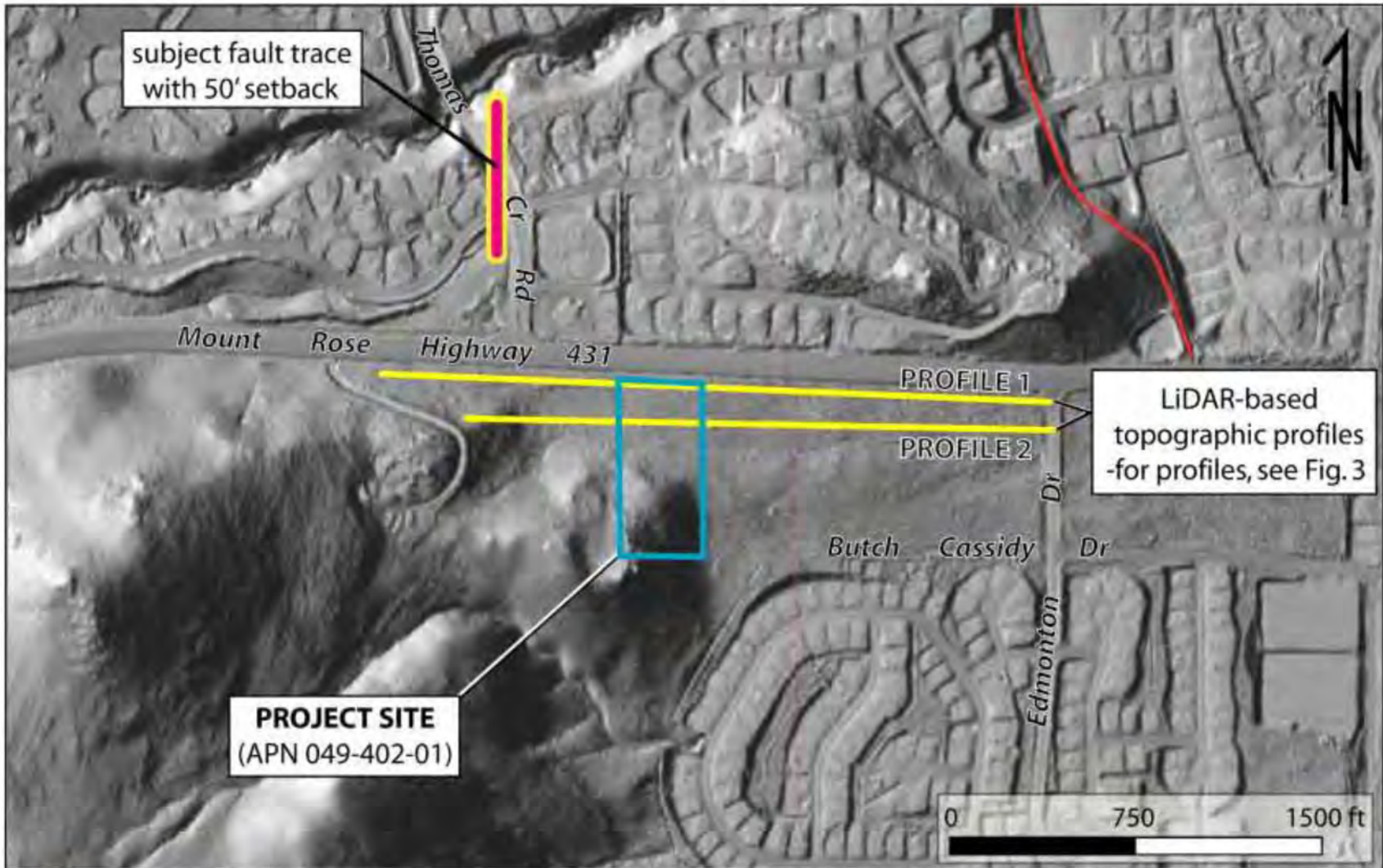
PIEDMONT GEOSEISMIC SERVICES, INC.

Thomas L. Sawyer
Seismic Hazard Specialist

REFERENCES CITED

- Bonham H.F. Jr., and Rogers, D.K., 1983, Mt. Rose NE Quadrangle Geologic Map: Nevada Bureau of Mines and Geology Map 4Bg, 1:24,000 scale.
- Nevada Bureau of Mines and Geology, 2023 (accessed), Interactive Quaternary fault map of Nevada: online resource; <https://gisweb.unr.edu/QuaternaryFaults/>.
- U.S. Geological Survey, 2023 (accessed), Quaternary Fault And fold database for the United States, web site: <http://earthquakes.usgs.gov/regional/qfaults>.



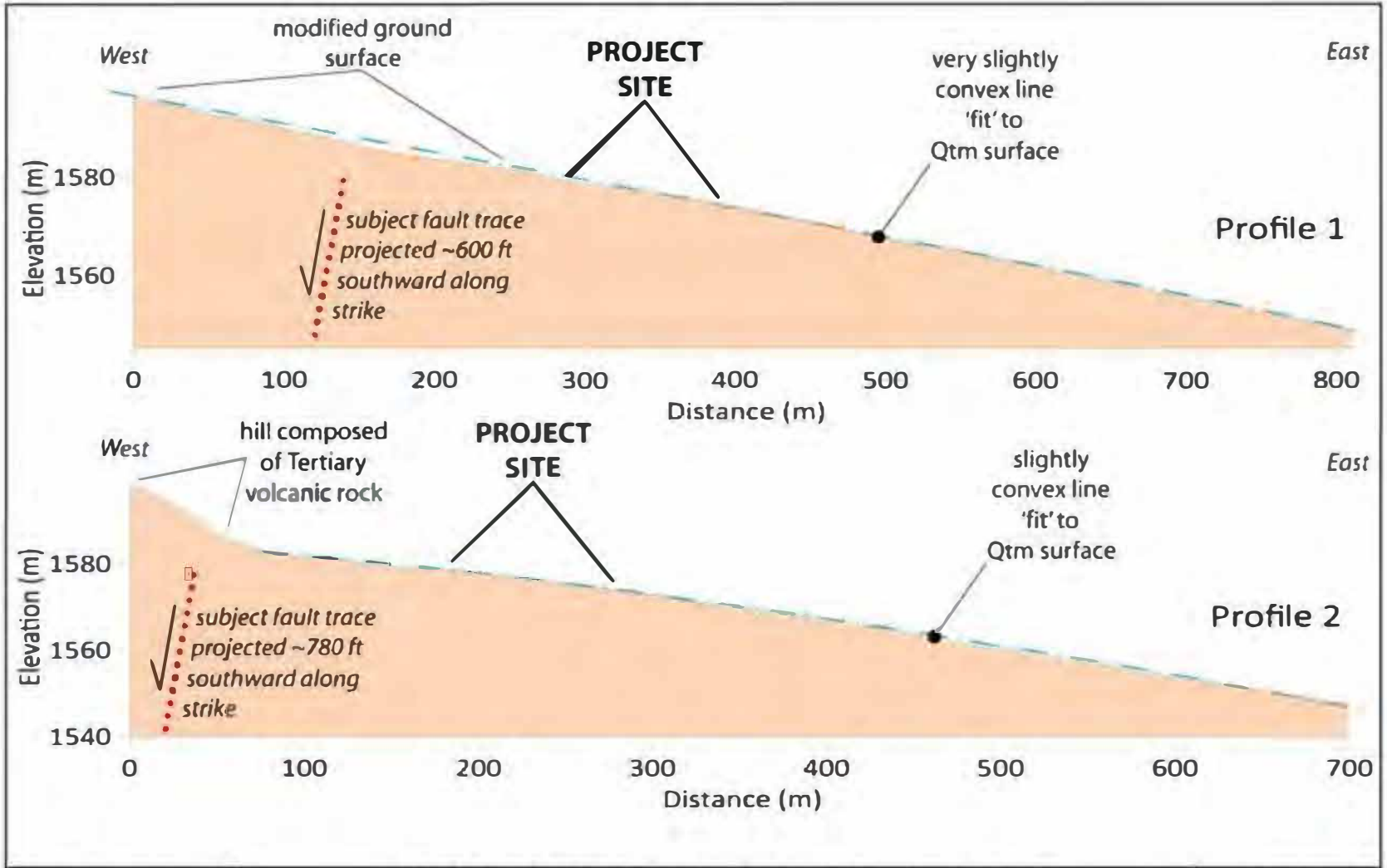


LiDAR-BASED SHADED-RELIEF MAP WITH QUATERNARY FAULTS
 APN 049-402-01 Quaternary Fault Investigation

FIGURE
2

GEO. BY TLS	SOURCE This study	SCALE As Reported	DATE 5/16/2023
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PROJECT NO.
2023-4100



LiDAR-BASED TOPOGRAPHIC PROFILES
 APN 049-402-01 Quaternary Fault Investigation

FIGURE
3

GEO. BY TLS	SOURCE This study	SCALE n/a	DATE 6/15/2023	PROJECT NO. 2023-4100
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August 7, 2023

Mitch Fink
 Community Services
 Washoe County
 1001 E 9th Street
 Reno, NV 89512

RE: Mount Rose CPD Mini-Warehouse Traffic Entry and Access Study

This letter has been prepared to provide trip generation calculations for the proposed Mount Rose Mini-Warehousing Storage Development located southwest of Mount Rose Highway (State Route 431) and Thomas Creek Road. The project site is within APN 049-402-01 in Reno, Nevada. A site plan is attached in **Attachment A**.

Trip Generation

For purposes of estimating trip vehicles that are anticipated to be generated by the project, the ITE Trip Generation Manual, 11th Edition (ITE Land Use Code 151 – Mini-Warehousing) was used. The ITE Trip Generation Manual informational report is a standard reference used by jurisdictions throughout the country and is based on actual trip generation studies performed at numerous locations in areas of various populations throughout the US.

Table 1 summarizes the estimated trips for a mini-warehousing facility project trips using ITE trip generation rates. Per the ITE Trip Generation Manual, the proposed facility would be expected to generate 7 AM project trips and 10 PM project trips based on a total of 583 storage units. The estimated daily trip totals is 105 vehicles per day. A project that generates less 80 peak hour trips does not require a traffic impact study with Washoe County. A detailed breakdown of trip generation calculations is available in **Attachment B**.

Table 1 – ITE Rate Project Trip Generation

Land Use	Size (Units)	AM Peak Hour Trips	PM Peak Hour Trips	Daily Trips
Mini-Warehousing	583 (Storage Units)	7	10	105
Total		7	10	105

Source: Institute of Transportation Engineers (ITE) Trip Generation Manual, 11th Edition

DRIVEWAY CAPACITY ANALYSIS

Synchro 11 was used to analyze the project driveways for Level of Service (LOS). Synchro is an interactive computer program that enables planners and engineers to forecast the traffic impacts of new developments; conduct area-wide traffic forecasting studies; test different mitigation measures; and compare different traffic scenarios. Synchro 11 utilizes HCM 6 methodology to analyze intersection delay and LOS.

Existing AM and PM peak hour turning movement data was field counted on Tuesday, July 25, 2023 at SR 431 and Thomas Creek Road. Traffic volumes at the future access drive to the project site was determined using the ITE Trip Generation Manual. The project site will have one right-in/right-out access driveway that will allow vehicles to ingress and egress the site via SR 431. The LOS for each access drive is summarized in **Table 2**. Calculations are attached.

Table 1 – Peak Hour LOS Analysis

Intersection	Existing + Project		2043 Background + Project	
	AM	PM	AM	PM
	Delay (LOS)	Delay (LOS)	Delay (LOS)	Delay (LOS)
SR 431 and Project Access Driveway Two-Way Stop Controlled Northbound Right	11.3 (B)	12.1 (B)	12.1 (B)	13.2 (B)

As shown in **Table 2**, the project access drive intersection is anticipated to have acceptable LOS at project completion and 20 years into the future. The LOS report is shown in **Attachment C**.

Access Spacing Standards

Chapter 4: Design Standards and Specifications of the Nevada Department of Transportation (NDOT) Access Management System and Standards 2017 Edition details design and construction standards and specifications for access to sites via major and minor roadways. Table 4-1 of the NDOT Access Management System and Standards specifies access spacing standards based on roadway class (i.e., interstate freeways, expressways, arterials, and collectors), posted speed limits, and access scenarios (i.e., full access or limited access). Information from Table 4-1 is summarized in **Table 3**.

Table 3 – Summary of NDOT Access Spacing Standards for Project Site

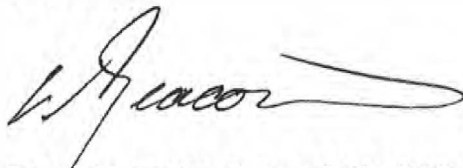
Roadway Class	Location/ Posted Speed Limit	Full Access		Limited Access	
		Signalized Intersection Uniform Spacing	Unsignalized Intersection/ Roundabout Minimum Spacing	Left-in/Right- in/Right-out only Minimum Spacing	Right-in/ Right-out only Minimum Spacing
4: Minor Arterials	≥ 40 mph	2,640'	1,320'	660'	350' - 600'

Source: Nevada Department of Transportation Access Management System and Standards, 2017 Edition

SR 431 is classified as a “minor arterial” and has a posted speed limit of 55 miles per hour (mph). According to **Table 3**, in order to implement full site access via a signalized intersection, the spacing between a proposed intersection and any future intersection must be 2,640 feet (ft). For full site access via an unsignalized intersection, the spacing between a proposed intersection and any future intersection must be 1,320ft. Limited access to the site via a Left-in/Right-in/Right-out only scenario requires 660 ft between any future Left-in/Right-in/ Right-out only scenarios. Limited access to the site via a Right-in/Right-out scenario requires 350 ft to 600 ft between any future Right-in/Right-out only scenarios. Due to proximity to the intersection of SR 431 and Thomas Creek Road, a Right-in/Right-out only project access drive on SR 431 should be constructed. Based on Table 4-18 in the NDOT Access Management System and Standards, for turning movements with less than five vehicles per hour on roadways above 45 mph in speed, a turn lane “May be Required.” Due to the downhill grade of SR 431 at that location, it is recommended that a right-turn turn deceleration lane be installed.

Please contact me at 775-200-1981 or David.Giacomin@kimley-horn.com should you have any questions regarding this analysis.

Sincerely,



David Giacomin, P.E., PTOE, RSP1
Project Engineer

Attachments

- Attachment A – Site Plan
- Attachment B – Trip Generation Calculations
- Attachment C – LOS Calculations



ATTACHMENT A

Site Plan

ATTACHMENT B

Trip Generation Calculations

Mini-Warehouse (151)

Vehicle Trip Ends vs: Occupied Storage Units (100s)
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

Avg. Num. of Occupied Storage Units (100s): 9

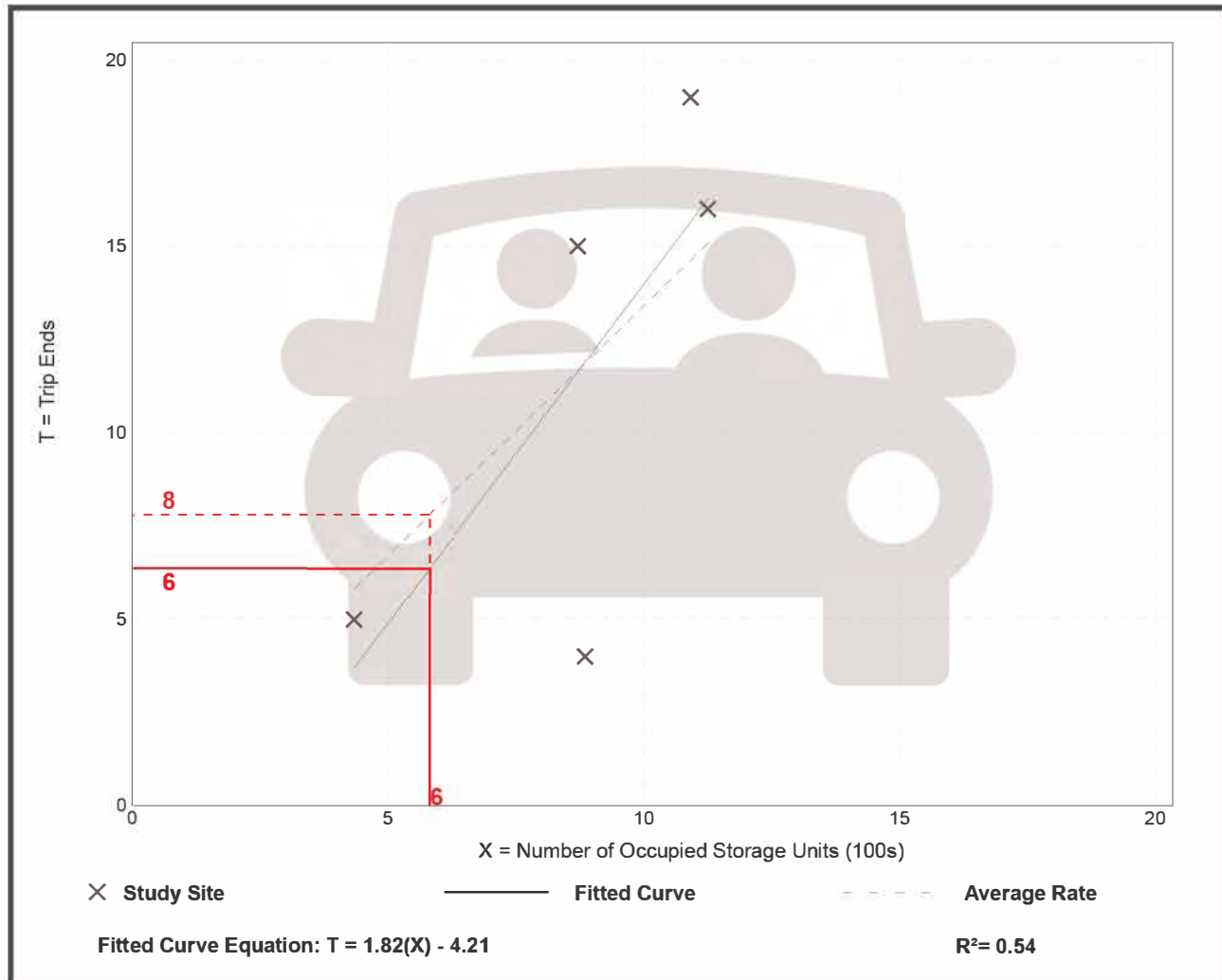
Directional Distribution: 56% entering, 44% exiting

Vehicle Trip Generation per Occupied Storage Unit (100s)

Average Rate	Range of Rates	Standard Deviation
1.34	0.45 - 1.74	0.54

Data Plot and Equation

Caution – Small Sample Size



Mini-Warehouse (151)

Vehicle Trip Ends vs: Occupied Storage Units (100s)
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

Avg. Num. of Occupied Storage Units (100s): 9

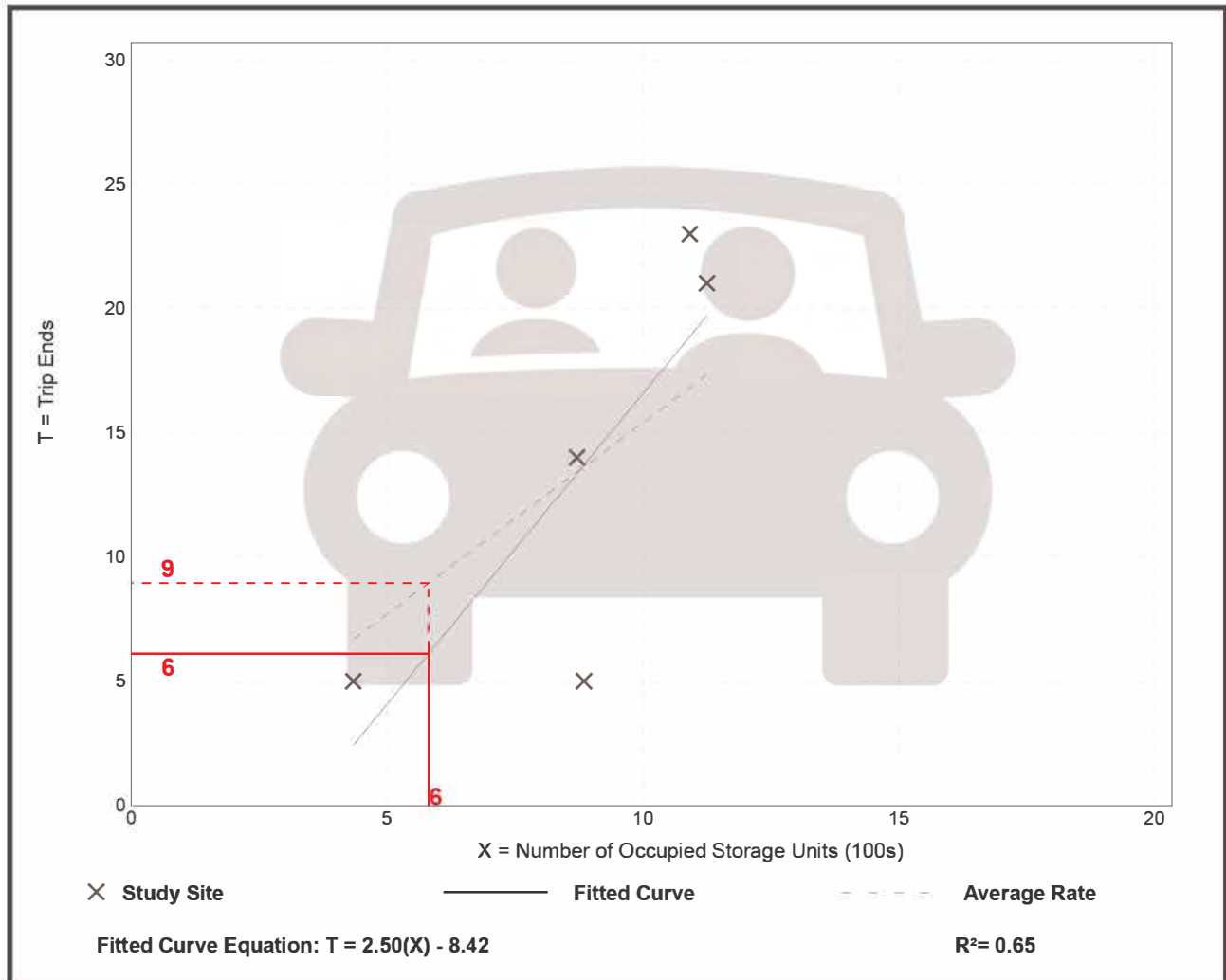
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Occupied Storage Unit (100s)

Average Rate	Range of Rates	Standard Deviation
1.54	0.56 - 2.11	0.63

Data Plot and Equation

Caution – Small Sample Size



Mini-Warehouse (151)

Vehicle Trip Ends vs: Occupied Storage Units (100s)
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 4

Avg. Num. of Occupied Storage Units (100s): 9

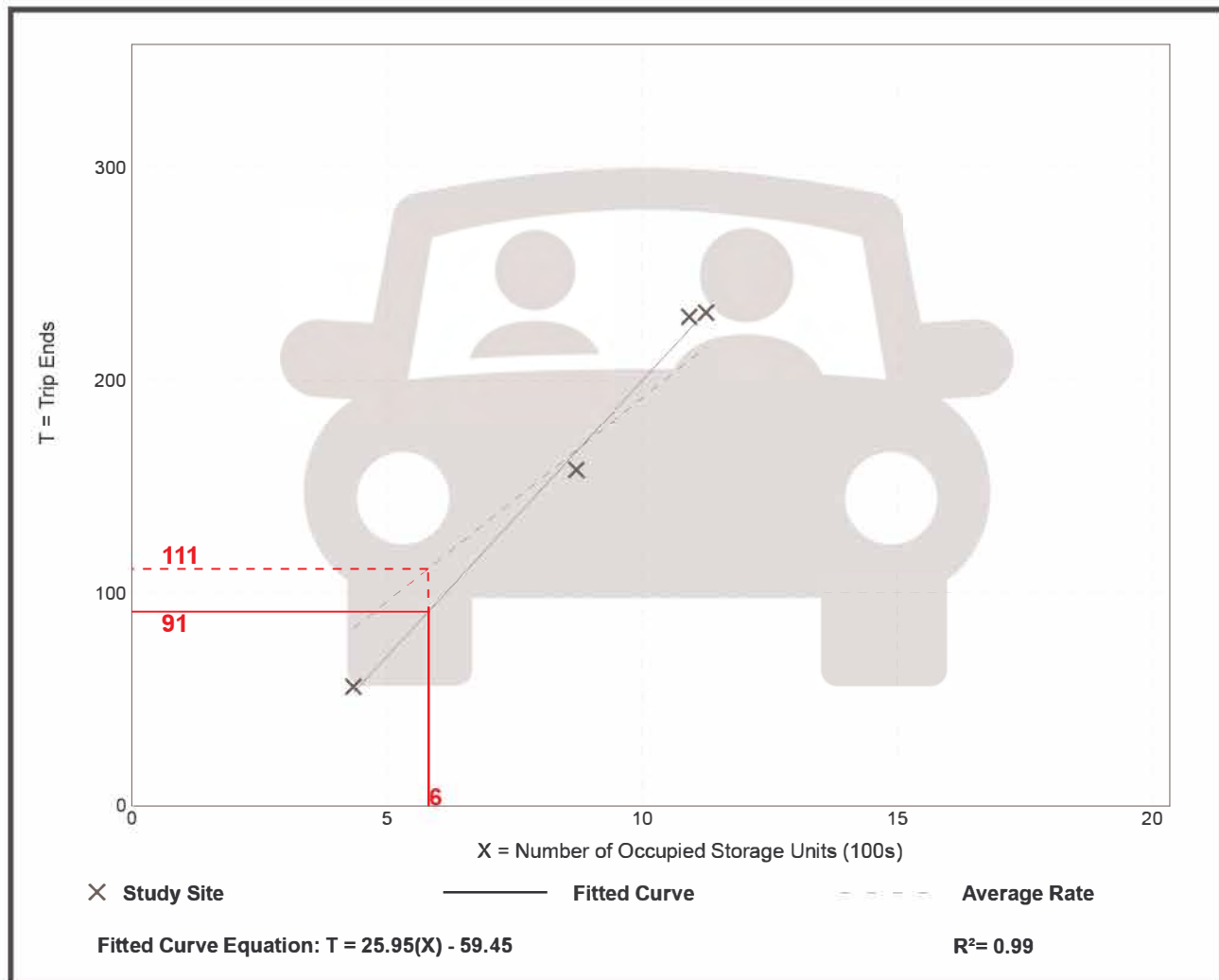
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Occupied Storage Unit (100s)

Average Rate	Range of Rates	Standard Deviation
19.19	12.90 - 21.06	3.03

Data Plot and Equation

Caution – Small Sample Size



ATTACHMENT C

LOS Calculations

HCM 6th TWSC
2: Project Access Driveway & SR 431

08/07/2023

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑		↑↑		↑
Traffic Vol, veh/h	803	4	0	717	0	3
Future Vol, veh/h	803	4	0	717	0	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	854	4	0	763	0	3

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	11.3
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT
Capacity (veh/h)	576	-	-	-
HCM Lane V/C Ratio	0.006	-	-	-
HCM Control Delay (s)	11.3	-	-	-
HCM Lane LOS	B	-	-	-
HCM 95th %tile Q(veh)	0	-	-	-

HCM 6th TWSC
2: Project Access Driveway & SR 431

08/07/2023

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑		↑↑		↑
Traffic Vol, veh/h	944	5	0	871	0	5
Future Vol, veh/h	944	5	0	871	0	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1015	5	0	937	0	5

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	-	-	- 508
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	-	0	-	0 510
Stage 1	-	-	0	-	0
Stage 2	-	-	0	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	510
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	12.1
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT
Capacity (veh/h)	510	-	-	-
HCM Lane V/C Ratio	0.011	-	-	-
HCM Control Delay (s)	12.1	-	-	-
HCM Lane LOS	B	-	-	-
HCM 95th %tile Q(veh)	0	-	-	-

HCM 6th TWSC
2: Project Access Driveway & SR 431

08/07/2023

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑		↑↑		↑
Traffic Vol, veh/h	949	4	0	847	0	3
Future Vol, veh/h	949	4	0	847	0	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	94	94	94	94	94	94
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1010	4	0	901	0	3
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	-	-	-	505
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	-	0	-	0	512
Stage 1	-	-	0	-	0	-
Stage 2	-	-	0	-	0	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	-	512
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	WB	NB			
HCM Control Delay, s	0	0	12.1			
HCM LOS						B
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT		
Capacity (veh/h)	512	-	-	-		
HCM Lane V/C Ratio	0.006	-	-	-		
HCM Control Delay (s)	12.1	-	-	-		
HCM Lane LOS	B	-	-	-		
HCM 95th %tile Q(veh)	0	-	-	-		

HCM 6th TWSC
2: Project Access Driveway & SR 431

08/07/2023

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑		↑↑		↑
Traffic Vol, veh/h	1116	5	0	1029	0	5
Future Vol, veh/h	1116	5	0	1029	0	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1200	5	0	1106	0	5
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	-	-	-	600
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	-	3.32
Pot Cap-1 Maneuver	-	-	0	-	0	444
Stage 1	-	-	0	-	0	-
Stage 2	-	-	0	-	0	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	-	444
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	WB	NB			
HCM Control Delay, s	0	0	13.2			
HCM LOS						B
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT		
Capacity (veh/h)	444	-	-	-		
HCM Lane V/C Ratio	0.012	-	-	-		
HCM Control Delay (s)	13.2	-	-	-		
HCM Lane LOS	B	-	-	-		
HCM 95th %tile Q(veh)	0	-	-	-		



GENERAL NOTES

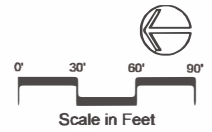
- ALL PLANTING AND IRRIGATION SHALL BE INSTALLED PER LOCAL GOVERNING CODES.
- TREES
 - DECIDUOUS TREES SHALL HAVE A MINIMUM CALIPER OF 2 INCHES.
 - EVERGREEN TREES SHALL HAVE A MINIMUM HEIGHT OF 7 FEET.
 - ADDITIONAL TREES, BEYOND THOSE REQUIRED BY CODE, MAY BE REDUCED IN SIZE AT INSTALLATION AND PROVIDED AT THE DISCRETION OF THE OWNER.
- FINAL PLANT SELECTION AND LAYOUT WILL BE BASED ON SOUND HORTICULTURAL PRACTICES RELATING TO MICRO-CLIMATE, SOIL, AND WATER REGIMES. ALL TREES WILL BE STAKED SO AS TO REMAIN UPRIGHT AND PLUMB FOLLOWING INSTALLATION. PLANT SIZE AND QUALITY AT TIME OF PLANTING WILL BE PER THE AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-1993).
- ALL SHRUB BEDS WILL RECEIVE 3" DEPTH MULCH WITH WEED CONTROL.
- ALL LANDSCAPING WILL BE AUTOMATICALLY IRRIGATED. CONTAINER PLANTINGS WILL BE DRIP IRRIGATED BASED ON THE SPECIFIC HORTICULTURAL REQUIREMENTS OF EACH SPECIES. A REDUCED-PRESSURE TYPE BACKFLOW PREVENTER WILL BE PROVIDED ON THE IRRIGATION SYSTEM AS REQUIRED PER CODE.
- PLANTING PLAN IS CONCEPTUAL. PLANT QUANTITIES INDICATED ARE PER LOCAL AGENCY CODE REQUIREMENTS. PLANT LOCATIONS, FINAL SPECIES SELECTION, AND SIZE AT PLANTING SHALL BE DETERMINED DURING DEVELOPMENT OF FINAL CONSTRUCTION DOCUMENTS.

LANDSCAPE DATA

SITE AREA: 218,333 SQ FT (5.026 ACRES)
 JURISDICTION: WASHOE COUNTY
 APN: 049 402 01
 ZONING: NC (NEIGHBORHOOD COMMERCIAL)
 AREA OF DEVELOPMENT IMPACT = 140,888 SQ. FT. (3.24 AC)
 AREA TO BE LEFT UNDEVELOPED = 79,237 SQ. FT. (1.82 AC)
 REQUIRED LANDSCAPE AREA = 28,188 SQ FT
 (20% OF TOTAL DEVELOPED AREA)
 PROVIDED LANDSCAPE AREA = 28,188 SQ FT MIN
 REQUIRED TREES = 96 MIN
 • (1 TREE PER 300 SQ FT OF REQUIRED LANDSCAPE AREA) = 94
 • (1 TREE PER 10 PARKING SPACES (7 PROVIDED)) = 1
 INC. LINES
 • (1 TREE PER 50 LN. FT. OF STREET FRONTAGE (320 LN FT)) = 8
 REQUIRED SHRUBS = 570 MIN
 • (6 SHRUBS PER REQUIRED TREE)

PLANT LEGEND

- ORNAMENTAL TREES
- DECIDUOUS SHADE TREES
- EVERGREEN TREES
- LANDSCAPE AREA
- RE-VEGETATION AREA



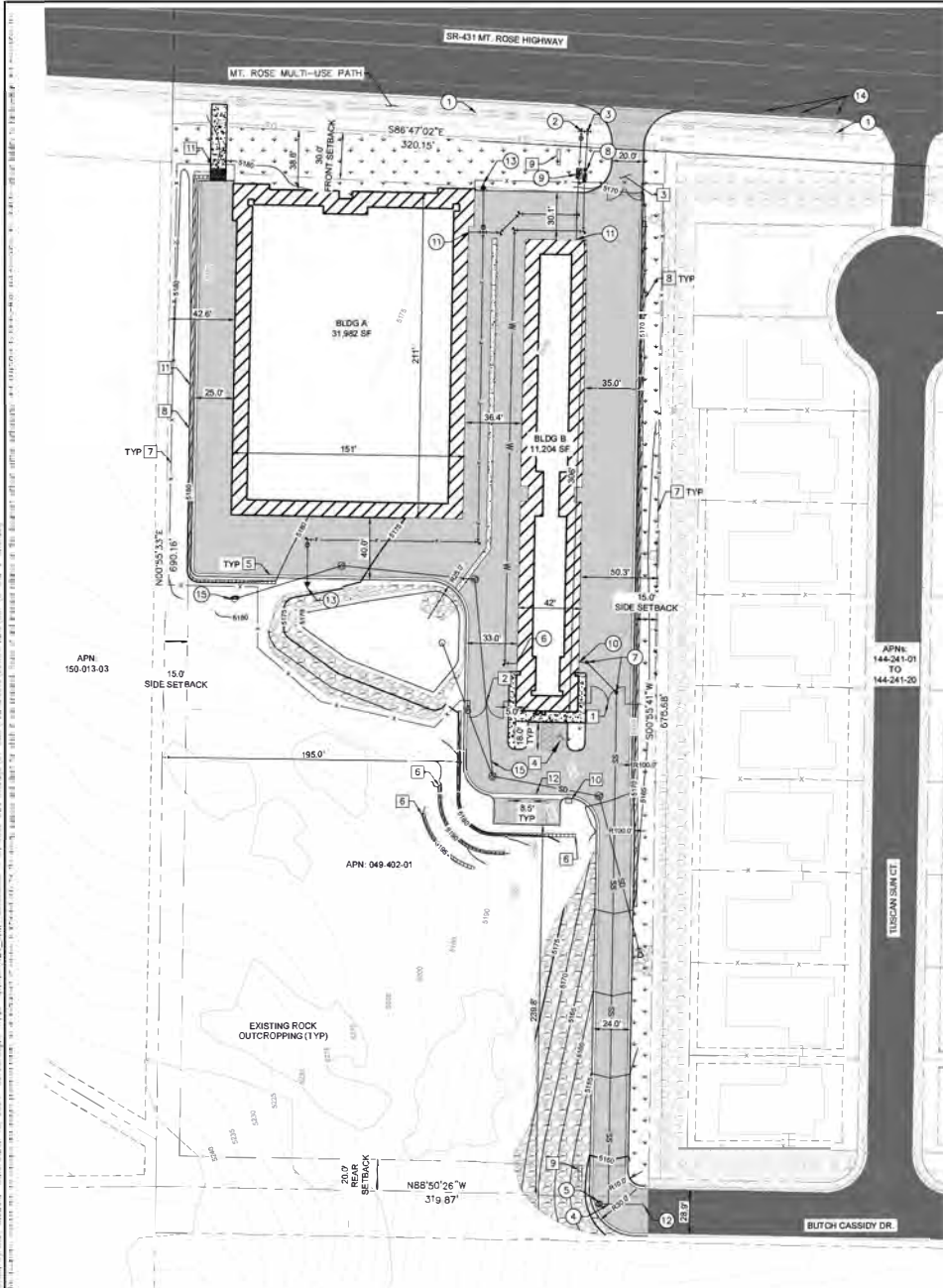
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L.A. Studio Nevada
 the landscape architecture studio
 1552 C Street Sparks, NV 89431 (775) 353-2223 NV RLA 4440
 www.la-studio.com



SUP Preliminary Landscape Plan
SUPREME STORAGE MT ROSE
 CPD Mt Rose, LLC

No.	Revision	Date

Sheet **L1** of **1**

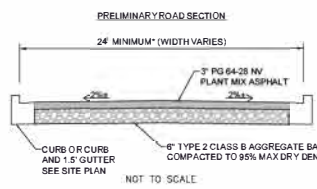


- NOTES:
1. THIS PLAN HAS BEEN DESIGN IN COMPLIANCE WITH ALL APPLICABLE PROVISIONS OF THE WASHOE COUNTY DEVELOPMENT CODE.
 2. CONTRACTOR TO VERIFY ALL EXISTING UTILITIES.
 3. NDOT MT. ROSE MULTI-USE PATH AND DRAINAGE IMPROVEMENTS (PROJ. NO. SPSR-043(024)) HAVE BEEN INCLUDED IN THE EXISTING BASEMAP OF THIS PLAN FOR REFERENCE ONLY. BASED ON CONSTRUCTION DRAWINGS PROVIDED BY NDOT. EXISTING SURFACE DOES NOT DEPICT NDOT IMPROVEMENTS. CONTRACTOR TO VERIFY EXISTING INFRASTRUCTURE PRIOR TO CONSTRUCTION. MAINTAIN 18" OF VERTICAL SEPARATION BETWEEN WATER AND SEWER MAINS CROSSINGS AS MEASURED FROM THE EXTERIOR WALLS OF THE PIPES PER NDEP REQUIREMENTS.
 4. MAINTAIN 10" OF HORIZONTAL SEPARATION BETWEEN WATER AND SEWER MAINS AS MEASURED FROM THE EXTERIOR WALLS OF THE PIPES PER NDEP REQUIREMENTS.
 5. ALL CURB RADI ARE 5' OR 20' FROM THE FACE OF CURB UNLESS NOTED OTHERWISE. ALL DIMENSIONS ARE FROM FACE OF CURB UNLESS NOTED OTHERWISE.
 6. THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) INDICATES THAT THE SITE IS LOCATED IN ZONE "X" UNSHADED OF FLOOD INSURANCE RATE MAP (FIRM) PANEL 32031C3245G AND 32031C3232G AS SHOWN ON THE FIRM INDEX, EFFECTIVE MARCH 16, 2009. ZONE "X" IS DEFINED BY FEMA AS "AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN."
 7. THERE ARE NO KNOWN POTENTIAL HAZARDS, ACTIVE FAULTS, SIGNIFICANT HYDROLOGICAL RESOURCES, OR WETLANDS WITHIN THE PROJECT AREA.
 8. SEE GRADING AND DRAINAGE PLAN FOR STORM WATER UTILITY INFORMATION.
 9. EXISTING AND PROPOSED TOPOGRAPHY ARE SHOWN PER THE SPECIAL USE PERMIT SITE PLAN REQUIREMENTS. SEE GRADING AND DRAINAGE PLAN.

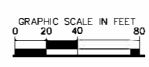
SITE INFORMATION	
SITE APN	049-402-01
SITE AREA	5.026 AC
ZONING	NEIGHBORHOOD COMMERCIAL
EXISTING LAND USE	100-VACANT/COMMERCIAL
PROPOSED LAND USE	500.GENERAL INDUSTRIAL
TOTAL PARKING REQUIRED	VARIANCE
TOTAL PARKING PROVIDED	7
ADA PARKING REQUIRED	1 (1 VAN)
ADA PARKING PROVIDED	1 (1 VAN)
CONSTRUCTION PHASING	1 PHASE
TOTAL NUMBER OF BUILDINGS	2

- UTILITY NOTES:
1. EXISTING NDOT TYPE 1 MODIFIED STORM DRAIN MANHOLE
 2. PROPOSED 6" DIP FIRE WATER CONNECTION TO EXISTING 12" C900 PVC WATER MAIN WITH GATE VALVE
 3. PROPOSED 1.5" C900 PVC DOMESTIC WATER CONNECTION TO EXISTING 12" C900 PVC
 4. PROPOSED 4" PVC SDR 35 SEWER LATERAL TO PROPOSED 8" PVC SDR 35 SEWER MAIN
 5. PROPOSED 48" SANITARY SEWER MANHOLE
 6. PROPOSED 1.5" DOMESTIC WATER BUILDING CONNECTION
 7. PROPOSED SANITARY SEWER CLEANOUT
 8. PROPOSED DOMESTIC WATER BACKFLOW PREVENTER AND SERVICE METER PER TMWA STANDARD DETAILS
 9. PROPOSED FIRE WATER BACKFLOW PREVENTER PER TMWA STANDARD DETAILS
 10. PROPOSED 4" SANITARY SEWER BUILDING CONNECTION
 11. PROPOSED FIRE WATER WATER BUILDING CONNECTION
 12. PROPOSED CONTINUATION OF EXISTING 8" PVC SDR 35 SEWER MAIN. MATCH EXISTING SLOPE
 13. PROPOSED FIRE HYDRANT PER WASHOE COUNTY STANDARD DETAILS
 14. EXISTING NDOT TYPE 2 DROP INLET
 15. PROPOSED STORM DRAIN. SEE GRADING AND DRAINAGE PLAN

- SITE NOTES:
1. PROPOSED VEHICLE ACCESS GATE FOR ENTRY
 2. PROPOSED VEHICLE ACCESS GATE FOR EXIT
 3. PROPOSED EMERGENCY ACCESS WITH GATE
 4. PROPOSED ADA STALL, SIGNAGE, AND ACCESS AISL
 5. PROPOSED TYPE 1 CURB AND GUTTER
 6. PROPOSED RETAINING WALL. SEE GRADING AND DRAINAGE PLAN
 7. PROPOSED PERIMETER FENCE
 8. PROPOSED LANDSCAPE WALL. SEE GRADING AND DRAINAGE PLAN
 9. PROPOSED MONUMENT SIGN
 10. PROPOSED POLE MOUNT LIGHT FIXTURE
 11. PROPOSED PEDESTRIAN EMERGENCY ACCESS WITH GATE
 12. PROPOSED PCC VALLEY GUTTER

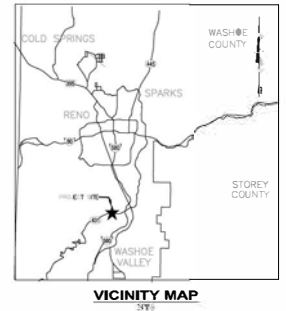


1. ROAD SECTION DESIGNED IN ACCORDANCE WITH WASHOE COUNTY STANDARDS.
 2. *EMERGENCY ACCESS WILL BE 20' WIDTH PER WASHOE COUNTY STANDARDS
 3. CUTFILL DAYLIGHT SLOPES VARY. REFER TO GRADING AND DRAINAGE PLAN
 4. THIS ROAD SECTION IS PRELIMINARY IN NATURE AND SUBJECT TO CHANGE. AT THIS TIME, A FINAL GEOTECHNICAL REPORT HAS NOT BEEN COMPLETED FOR THIS SITE.



LEGEND

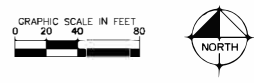
- PROPERTY LINE
- SETBACK LINE
- EXISTING EASEMENT
- EXISTING CONTOURS
- 5000 PROPOSED CONTOURS
- EXISTING BARRIER RAIL
- EXISTING WALL
- PROPOSED FENCE
- EXISTING WATER
- EXISTING ELECTRIC
- GAS
- EXISTING GAS
- EXISTING SANITARY SEWER
- EXISTING STORM DRAIN
- EXISTING FIBER OPTIC
- PROPOSED WATER LINE
- PROPOSED SANITARY SEWER
- PROPOSED STORM DRAIN
- PROPOSED FIRE WATER
- PROPOSED FIRE HYDRANT
- PROPOSED ASPHALT PAVEMENT
- EXISTING ASPHALT PAVEMENT
- EXISTING MULTI-USE PATH
- EXISTING SIDEWALK
- EXISTING RIPRAP
- PROPOSED LANDSCAPE AREA
- PROPOSED RIPRAP
- PROPOSED SIDEWALK



NO.	REVISIONS	DATE	BY
MT ROSE MINI STORAGE WASHOE, NV			
SITE AND UTILITY PLAN			
FOR REVIEW ONLY NOT FOR CONSTRUCTION			
SHEET NUMBER 1 OF 2			



- NOTES:
1. THIS PLAN HAS BEEN DESIGNED IN COMPLIANCE WITH ALL APPLICABLE PROVISIONS OF THE WASHOE COUNTY DEVELOPMENT CODE.
 2. CONTRACTOR TO VERIFY ALL EXISTING UTILITIES.
 3. NDOT MT. ROSE MULTI-USE PATH AND DRAINAGE IMPROVEMENTS (PROJ. NO. SPSR-043(024)) HAVE BEEN INCLUDED IN THE EXISTING BASEMAP OF THIS PLAN FOR REFERENCE ONLY. BASED ON CONSTRUCTION DRAWINGS PROVIDED BY NDOT. EXISTING SURFACE DOES NOT DEPICT NDOT IMPROVEMENTS. CONTRACTOR TO VERIFY EXISTING INFRASTRUCTURE PRIOR TO CONSTRUCTION.
 4. MAINTAIN 18" OF VERTICAL SEPARATION BETWEEN WATER AND SEWER MAINS CROSSINGS, AS MEASURED FROM THE EXTERIOR WALLS OF THE PIPES PER NDEP REQUIREMENTS.
 5. MAINTAIN 10" OF HORIZONTAL SEPARATION BETWEEN WATER AND SEWER MAINS, AS MEASURED FROM THE EXTERIOR WALLS OF THE PIPES PER NDEP REQUIREMENTS.
 6. THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) INDICATES THAT THE SITE IS LOCATED IN ZONE "X" UNSHADED OF FLOOD INSURANCE RATE MAP (FIRM) PANEL 32031C0245G AND 32031C03232G AS SHOWN ON THE FIRM INDEX, EFFECTIVE MARCH 16, 2009. ZONE "X" IS DEFINED BY FEMA AS "AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN."
 7. THERE ARE NO KNOWN POTENTIAL HAZARDS, ACTIVE FAULTS, SIGNIFICANT HYDROLOGICAL RESOURCES, OR WETLANDS WITHIN THE PROJECT AREA.
 8. SEE SITE PLAN FOR SITE AND UTILITY INFORMATION.



LEGEND

---	PROPERTY LINE
---	SETBACK LINE
---	EXISTING EASEMENT
---	EXISTING CONTOURS
---	PROPOSED CONTOURS
---	PROPOSED GRADING LIMITS
---	EXISTING BARRIER RAIL
---	EXISTING WALL
X	PROPOSED FENCE
W	EXISTING WATER
E	EXISTING ELECTRIC
GAS	EXISTING GAS
SD	EXISTING SANITARY SEWER
SD	EXISTING STORM DRAIN
FO	EXISTING CHANNEL FLOW LINE
FO	PROPOSED SWALE FLOW LINE
FO	EXISTING FIBER OPTIC
W	PROPOSED WATER LINE
SS	PROPOSED SANITARY SEWER
SD	PROPOSED STORM DRAIN
F	PROPOSED FIRE WATER
▲	PROPOSED FIRE HYDRANT
□	EXISTING MULTI-USE PATH
□	EXISTING SIDEWALK
□	EXISTING RIPRAP
□	PROPOSED SIDEWALK
□	PROPOSED LANDSCAPE AREA
□	PROPOSED RIPRAP
■	EXISTING SLOPES EXCEEDING 15%
■	EXISTING SLOPES EXCEEDING 30%

GRADING NOTES:

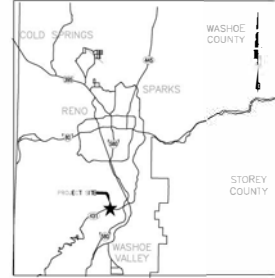
- 1 PROPOSED DRAINAGE CHANNEL
- 2 EXISTING PRIVATE DRAINAGE CHANNEL
- 3 PROPOSED RETAINING WALL, MAX HEIGHT 10'
- 4 PROPOSED 3" WIDE PCC VALLEY GUTTER
- 5 PROPOSED DETENTION BASIN (8,070 CY)
- 6 PROPOSED 2' CURB OPENING AND RIPRAP APRON
- 7 PROPOSED BEEHIVE GRATE OUTLET
- 8 PROPOSED 48" STORM DRAIN MANHOLE
- 9 PROPOSED STORM DRAIN FLARED END SECTION
- 10 PROPOSED 18" SDR 35 PVC STORM DRAIN PIPE
- 11 PROPOSED 3" CURB OPENING AND RIPRAP APRON
- 12 PROPOSED 3" CURB OPENING AND RIPRAP APRON
- 13 PROPOSED LANDSCAPE WALL, MAX HEIGHT 5'
- 14 PROPOSED 24" SDR 35 PVC STORM DRAIN PIPE
- 15 PROPOSED TYPE 1 CURB AND GUTTER

TEMPORARY EROSION CONTROL NOTES:

- 1 PROPOSED TEMPORARY STORM WATER INLET PROTECTION
- 2 PROPOSED TEMPORARY CONSTRUCTION ENTRANCE
- 3 PROPOSED TEMPORARY SILT FENCE TO BE INSTALLED ALONG LIMITS OF DISTURBANCE

EARTHWORK QUANTITIES

CUT = 9,234 CY
 FILL = 7,155 CY
 NET (EXPORT) = 2,079 CY
 *EARTHWORK VOLUMES BASED ON PRELIMINARY GRADING AND ARE SUBJECT TO CHANGE. VALUES PROVIDED PER WASHOE COUNTY REQUIREMENTS.
 NOT TO BE USED FOR ANY PURPOSE OTHER THAN PRELIMINARY REVIEW.



PRELIMINARY FOR REVIEW ONLY NOT FOR CONSTRUCTION

Call before you Dig
811
1-800-227-2600

NO.	REVISIONS	DATE	BY

Kimley-Horn

2024 KIMLEY-HORN AND ASSOCIATES, INC.
 7300 RANDOLPH PARKWAY, SUITE 100, RENO, NV 89501
 WWW.KIMLEY-HORN.COM

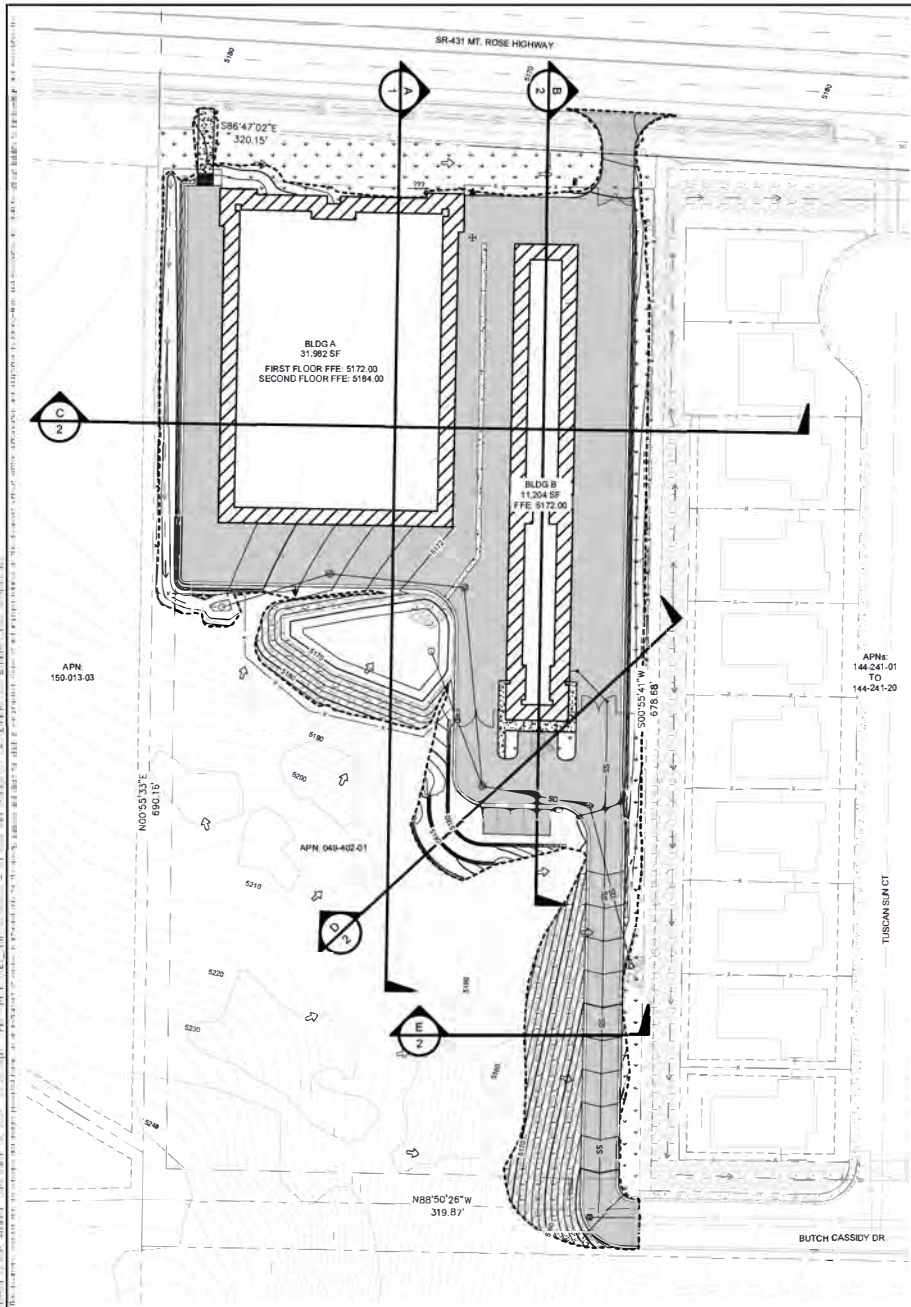
REGISTERED PROFESSIONAL ENGINEER - STATE OF NV
 No. 027550

KHVA PROJECT	DATE
19233500000	2024-03-19
SOURCE: AS SHOWN	DRAWN BY: JF, AW
CHECKED BY: JF, AW	CREATED BY: JF, AW

GRADING AND DRAINAGE SHEET

MT ROSE MINI STORAGE WASHOE, NV

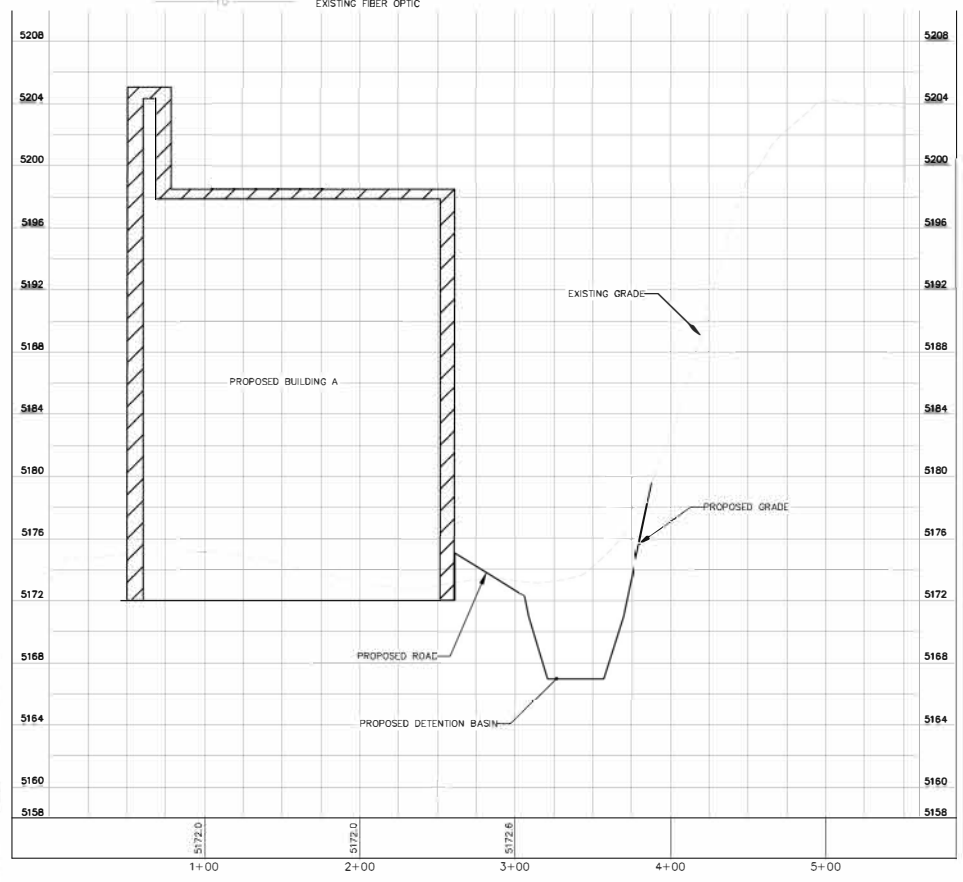
SHEET NUMBER	2 OF 2
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NOTES:
1. THIS PLAN HAS BEEN DESIGNED IN COMPLIANCE WITH ALL APPLICABLE PROVISIONS OF THE WASHOE COUNTY DEVELOPMENT CODE
2. SEE SITE PLAN AND GRADING PLAN FOR MORE INFORMATION

LEGEND

	PROPERTY LINE		PROPOSED WATER LINE
	SETBACK LINE		PROPOSED SANITARY SEWER
	EXISTING EASEMENT		PROPOSED STORM DRAIN
	EXISTING CONTOURS		PROPOSED FIRE WATER
	PROPOSED CONTOURS		PROPOSED SWALE FLOW LINE
	PROPOSED GRADING LIMITS		EXISTING MULTI-USE PATH
	EXISTING BARRIER RAIL		EXISTING SIDEWALK
	EXISTING WALL		EXISTING RIPRAP
	PROPOSED FENCE		PROPOSED SIDEWALK
	EXISTING WATER		PROPOSED LANDSCAPE AREA
	EXISTING SANITARY SEWER		PROPOSED RIPRAP
	EXISTING STORM DRAIN		
	EXISTING CHANNEL FLOW LINE		
	EXISTING FIBER OPTIC		



REVISIONS

No.	DATE	BY

Kimley Horn

2024 KIMLEY-HORN AND ASSOCIATES, INC.
7800 RANCHARABAT PARKWAY, SUITE 100, RENO, NV 89501
WWW.KIMLEY-HORN.COM

PROFESSIONAL ENGINEER - STATE OF NEVADA
BOB T. KIMLEY
No. 027590

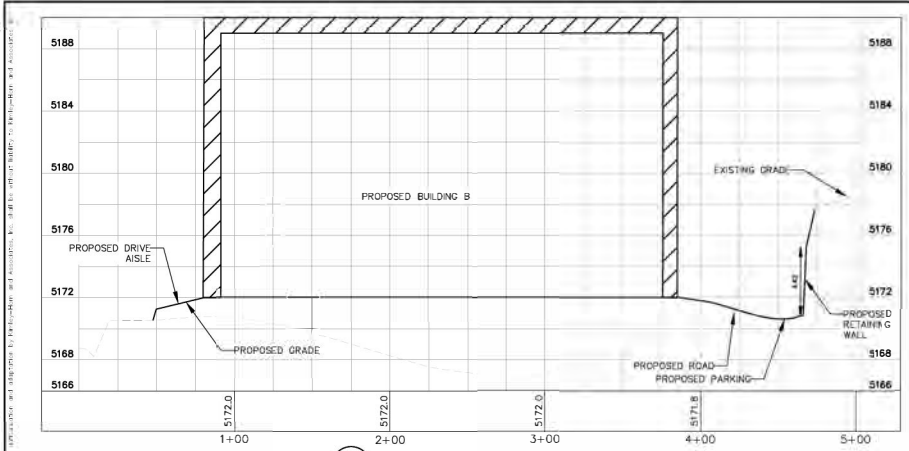
3/19/2024

PROJECT: MT ROSE MINI STORAGE
DATE: 2024-03-19
SCALE: AS SHOWN
DRAWN BY: JT, AW
CHECKED BY: CW

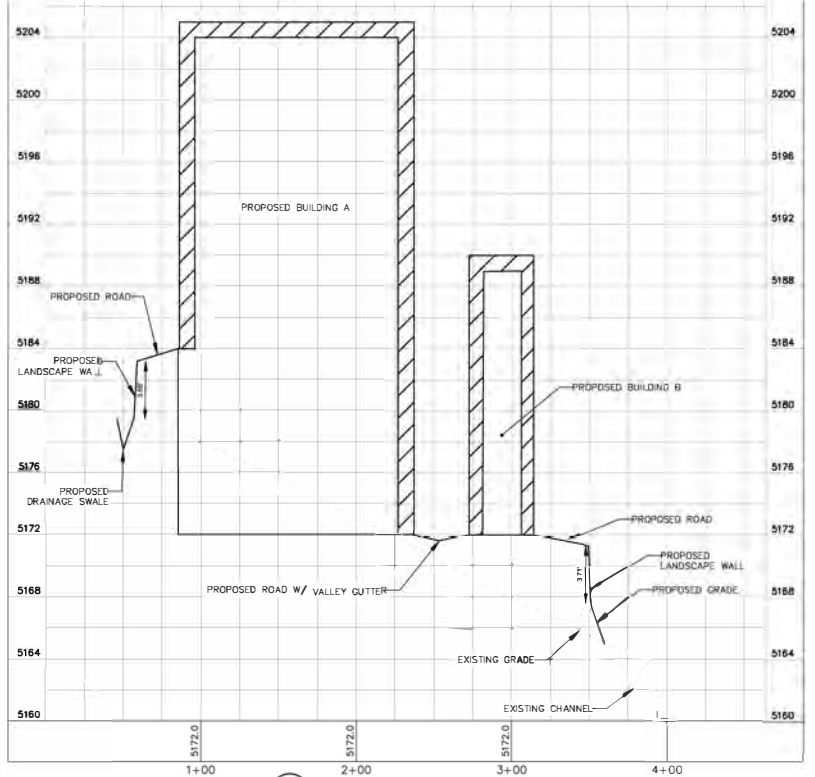
PRELIMINARY SECTIONS

MT ROSE MINI STORAGE
WASHOE, NV

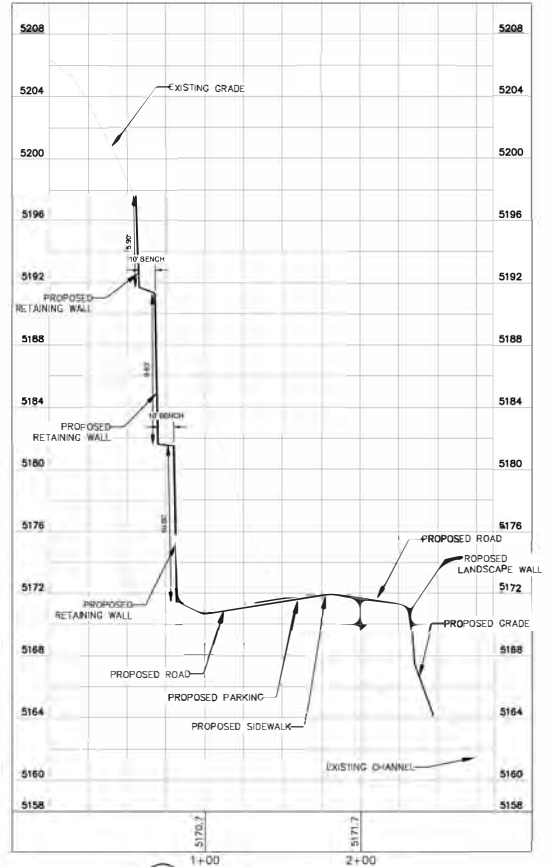
SHEET NUMBER
1 OF 2



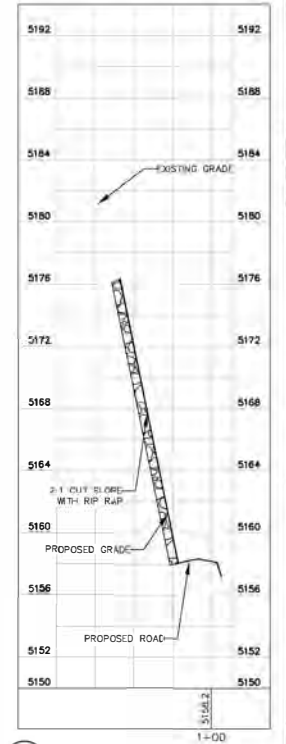
B
SECTION B
HORIZ. SCALE: 1" = 40' VERT. SCALE 1" = 4'



C
SECTION C
HORIZ. SCALE: 1" = 40' VERT. SCALE 1" = 4'



D
SECTION D
HORIZ. SCALE: 1" = 40' VERT. SCALE 1" = 4'



E
SECTION E
HORIZ. SCALE: 1" = 40' VERT. SCALE 1" = 4'

NOTES:
1. THIS PLAN HAS BEEN DESIGNED IN COMPLIANCE WITH ALL APPLICABLE PROVISIONS OF THE WASHOE COUNTY DEVELOPMENT CODE
2. SEE SITE PLAN AND GRADING PLAN FOR MORE INFORMATION

NO.	REVISIONS	DATE	BY

Kimley»Horn
© 2024, KIMLEY-HORN AND ASSOCIATES, INC.
7700 RANCHARABAT PARKWAY, SUITE 100, REDDING, NV 89811
WWW.KIMLEY-HORN.COM

PROFESSIONAL ENGINEER - STATE OF NEVADA
REGISTERED PROFESSIONAL ENGINEER
NO. 027538

DATE: 2/27/24
SCALE: AS SHOWN
DRAWN BY: JF/AW
CHECKED BY: JF/AW

PRELIMINARY SECTIONS

MT ROSE MINI STORAGE WASHOE, NV

SHEET NUMBER
1 OF 2

MOUNT ROSE MINI STORAGE

Truck Turn Exhibit

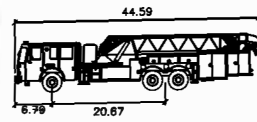
Washoe County

March 2024

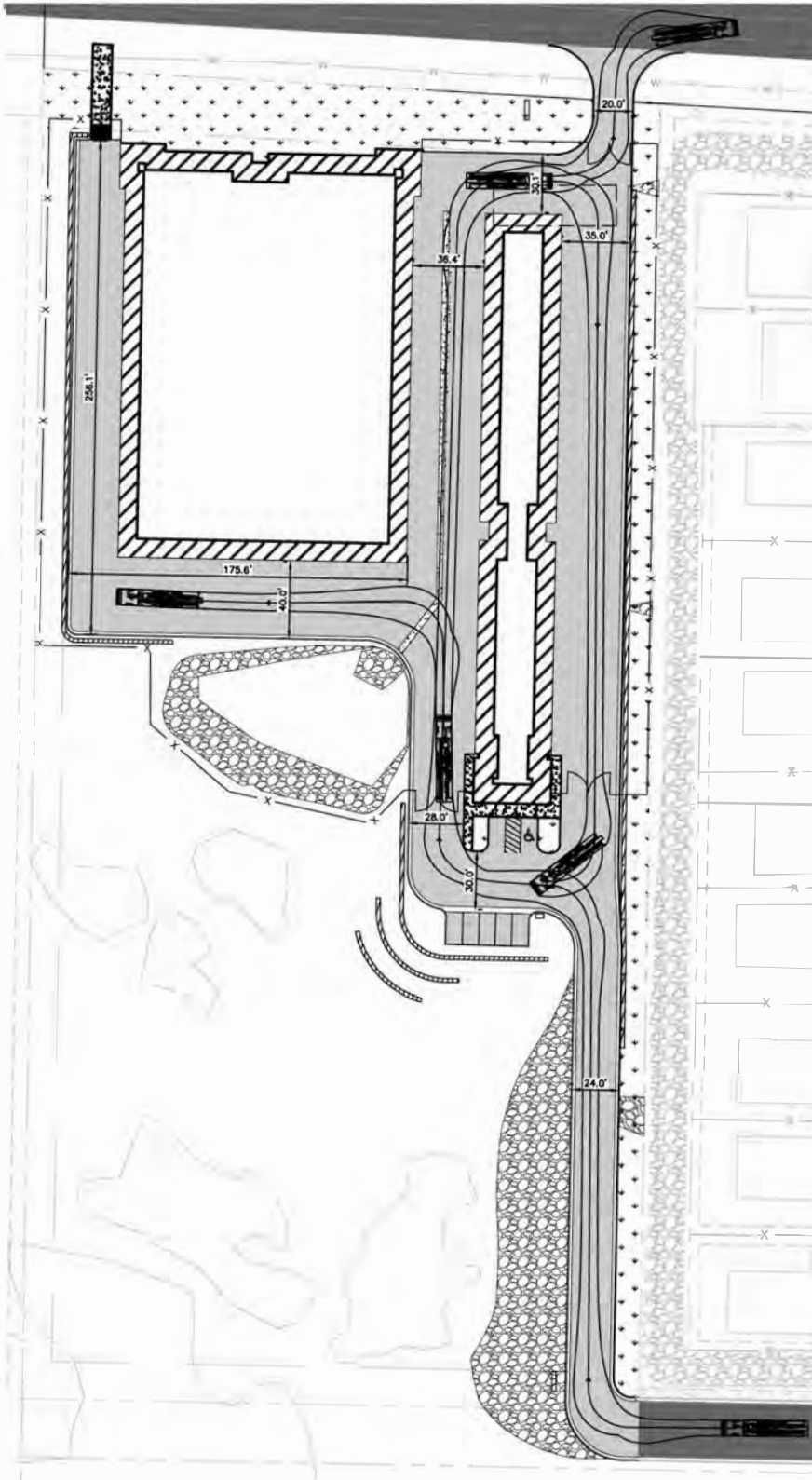
NOTE: THIS PLAN IS CONCEPTUAL IN NATURE



GRAPHIC SCALE IN FEET



Smeal Aerial MM 100ft	
	feet
Width	: 8.17
Track	: 7.75
Lock to Lock Time	: 6.0
Steering Angle	: 45.0



SUPREME STORAGE MT. ROSE

3900 MOUNT ROSE HWY RENO, NV 89511
SPECIAL USE PERMIT
MARCH 20, 2024



SHEET INDEX

SHEET NO.	SHEET NAME	DATE	BY	CHKD.
00	COVER SHEET			
CIVIL				
C01	1. SITE ANALYSIS/TV PLAN			
C02	2. SANITARY AND SEWERAGE PLAN			
C03	3. VENTILATION EXHAUST PLAN			
C04	4. DEVELOPMENT AND SITE LAYOUT			
C05	5. CONCRETE FOUNDATION EXHAUST			
C06	6. FLOOD ANALYSIS EXHAUST			
C07	7. SOIL FILL ANALYSIS EXHAUST			
LANDSCAPE				
L01	1. PRELIMINARY LANDSCAPE PLAN			
ARCHITECTURAL				
A01	1. SITE PLAN			
A02	2. PARK ACCESS PLAN			
A03	3. EXTERIOR SITE ELEVATIONS			
A04	4. EXTERIOR ELEVATIONS - BUILDING A			
A05	5. EXTERIOR ELEVATIONS - BUILDING B			
ELECTRICAL				
E01	1. SITE LIGHTING PLAN			
E02	2. SITE ELECTRICAL PLAN			



JACKSON | MAIN
ARCHITECTURE & CONSTRUCTION
3111 1ST AVE. SUITE 100
RENO, NV 89501
TEL: 775.784.8800
WWW.JACKSONMAIN.COM

SUPREME STORAGE MT. ROSE | CPD MT. ROSE, LLC.
15115 NE 67TH PL., REDMOND, WA. 98052
3900 MOUNT ROSE HWY
RENO, NV 89511



VICINITY MAP
N.T.S.



LOCATION MAP
N.T.S.

LEGAL DESCRIPTION

T34 LOT NO. 048-001-01
ZONING: NC - NEIGHBORHOOD COMMERCIAL

PROJECT DESCRIPTION

PROPOSED NEW STORAGE PROJECT CONSISTING OF TWO BUILDINGS, BUILDING A MEASURING 100' WITH TWO STORES, BUILDING B MEASURING 100' WITH ONE STORE. BOTH BUILDINGS ARE TYPE BA CONSTRUCTION.

OWNER
CPD MT. ROSE, LLC
15115 NE 67TH PL.
REDFORD, WA 98052
PHONE: (509) 822-5550
EMAIL: JSTFORDEL@CPD.MOUNTROSE.COM
CONTACT: JOSEPH STROBLE

ARCHITECT
JACKSON | MAIN ARCHITECTURE P.S.
3111 1ST AVE. S.
SEATTLE, WA 98104
PHONE: (206) 324-8000
EMAIL: DAVID@JACKSONMAIN.COM
CONTACT: DAVID HARRIS
ARCHITECT: ROBIN MURPHY

LANDSCAPE ARCHITECT
L.A. STUDIO NEVADA
1525 O STREET
SPARKS, NV 89411
PHONE: (775) 323-2223
EMAIL: INFO@L.A.STUDIO.NV.COM
CONTACT: MARC CHARPILLE

STRUCTURAL ENGINEER
HW ENGINEERING
1001 WEST PLANNING ROAD, SUITE 100
LAS VEGAS, NV 89147
PHONE: (702) 200-0061
EMAIL: YANCHAO@HWENGINEERINGUSA.COM
CONTACT: YANSHAO

ELECTRICAL ENGINEER
MARC SIMONONI ELECTRICAL ENGINEERING
466 SUMMIT DR.
RENO, NV 89502
PHONE: (775) 737-9345
EMAIL: MSELTR@AOL.COM
CONTACT: MARC SIMONONI

MECH/PLUMBING ENGINEER
SCF ENGINEERING
3450 FORTYFIFTH CT.
RENO, NV 89521
PHONE: (775) 501-4123
EMAIL: DPREY@SCFENG.COM
CONTACT: SEAN G. PREY

CIVIL ENGINEER
KIMLEY-HORN
7300 SAGEHURST PARKWAY SUITE 100
RENO, NV 89501
PHONE: (775) 460-9100
EMAIL: CHRIS.VINCHER@KIMLEY-HORN.COM
CONTACT: CHRIS VINCHESTER

NO.	DATE	DESCRIPTION
1	03-20-2024	SPECIAL USE PERMIT

PROJECT NO.: 23199
PROJECT NAME: SH
DRAWN BY: AS
CHECKED BY: RA

COVER SHEET

GO.00

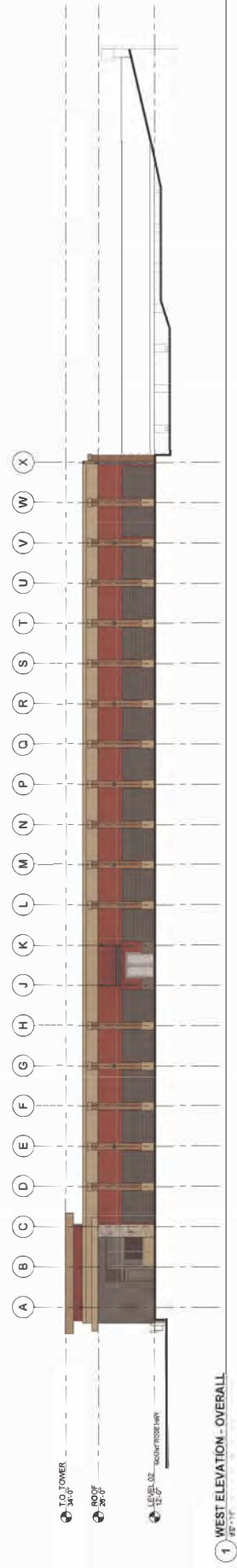
WSUP23-0027
EXHIBIT D

NO.	DATE	DESCRIPTION
1	10/20/20	ISSUED FOR PERMIT
2	11/15/20	ISSUED FOR PERMIT

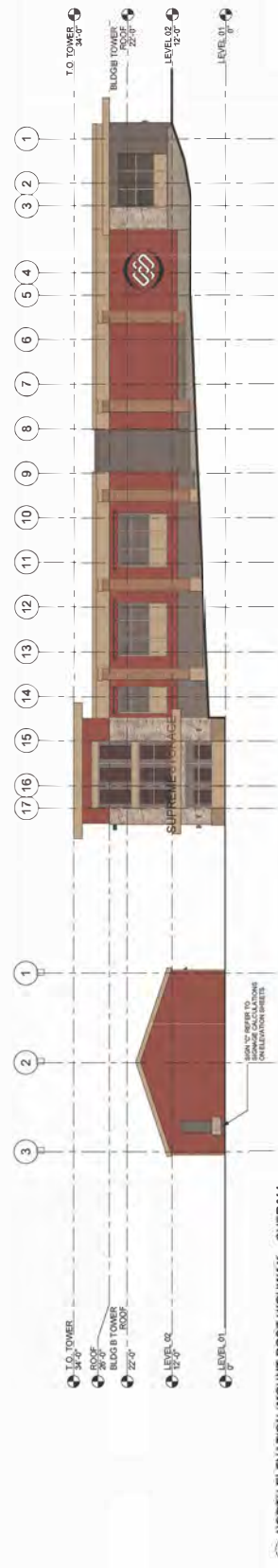
PROJECT NO.	DATE
2020-0027	10/20/20
DRAWN BY:	CM
CHECKED BY:	DM

EXTERIOR SITE ELEVATIONS

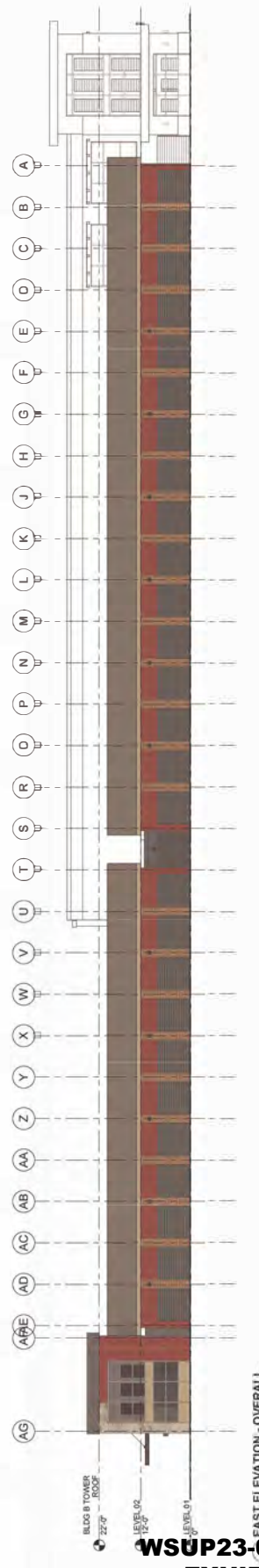
A3.01



1 WEST ELEVATION - OVERALL
 1/8" = 1'-0"



2 NORTH ELEVATION (MOUNT ROSE HIGHWAY) - OVERALL
 1/8" = 1'-0"



EAST ELEVATION - OVERALL
 1/8" = 1'-0"



JACKSON IMAI
ARCHITECTURAL
15115 NE 67TH PL, REDMOND, WA 98052
PH: 206.881.1111
WWW.JIMARCHITECTURAL.COM

CPD MT. ROSE, LLC
15115 NE 67TH PL, REDMOND, WA, 98052

SUPREME STORAGE MT ROSE
3900 MOUNT ROSE HWY
RENO, NV 89511

NO.	DESCRIPTION
1	100% SCAFFOLD
2	100% SCAFFOLD
3	100% SCAFFOLD
4	100% SCAFFOLD
5	100% SCAFFOLD
6	100% SCAFFOLD
7	100% SCAFFOLD
8	100% SCAFFOLD
9	100% SCAFFOLD
10	100% SCAFFOLD

PROJECT NO: 2009
DATE: 08/20/2023
DRAWN BY: JIM
CHECKED BY: JIM
EXTERIOR ELEVATIONS - BUILDING B
A3.03

SHEET NOTES:

MATERIAL LEGEND:

PRODUCT	COLOR	MANUFACTURER
1	1	1
2	2	2
3	3	3
4	4	4
5	5	5
6	6	6
7	7	7
8	8	8
9	9	9
10	10	10

NOTE: ALL COLORS INDICATED ARE SUBJECT TO CHANGE

KEYNOTES:

NOTE

EXTERIOR SIGNAGE INFO:

SEE ARTICLE FOR SIGNAGE AND TABLE 110506.11.1
MAX. SIGNAGE TO BE 10% OF EACH FLOOR OF RP)
MAX. SIGNAGE ALLOWABLE IS 30%
SIGN AREA PROVIDED: 33.75

SIGN B:
SIGN AREA: 100% OF RP
SIGN DIMENSIONS: 10' 0" x 7' 0"
SIGN MOUNTING: 10' 0" OF
SIGN STYLE: TORQUEMENT SIGN

SIGN C:
SIGN AREA: 100% OF RP
SIGN DIMENSIONS: 10' 0" x 7' 0"
SIGN MOUNTING: 10' 0" OF
SIGN STYLE: TORQUEMENT SIGN

